



PROJECT DEVELOPMENT REPORT



CDOT No. E-5-482
Section No. 16-E5482-00-BT
Cook County, Illinois

PREPARED FOR:

City of Chicago
Department of Planning and
Development
121 N LaSalle Street
Chicago, IL 60602

City of Chicago
Department of Transportation
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PREPARED BY:



Table of Contents – Volume 1

Local Project Development Report (IDOT BLR Form 22210)

1. Location and Existing Conditions	1
2. Proposed Improvement.....	5
3. Crash Analysis (BLRS Manual Section 22-2.11(b)(9)).....	8
4. Right-of-Way.....	8
5. Prime Farmland (BLRS Manual Section 20-10)	9
6. Floodplain Encroachment (BLRS Manual Section 20-7)	9
7. Phase I & II NPDES Storm Water Permit Requirements (BLRS Manual Section 7-4.01)	9
8. “404” Permit (BLRS Manual Section 7-4.02).....	10
9. Special Waste (BLRS Manual Section 20-12)	10
10. Environmental Survey (BLRS Manual Section 20-2)	11
11. Section 4(f) Lands (BLRS Manual Section 20-3)	11
12. Air Quality (BLRS Manual Section 20-11) Check One:.....	12
13. Noise (BLRS Manual Section 20-6).....	13
14. Work Zone Transportation Management Plans	14
15. Complete Streets (BLRS Manual Chapter 10).....	14
16. Maintenance of Traffic (BLRS Manual Section 22-2.11(b)(9))	14
17. Public Involvement (BLRS Manual Chapter 21)	15
18. Coordination: LA-IDOT-FHWA (BLRS Manual Section 22-1.02)	16
19. Other Coordination.....	16
20. Summary of Commitments.....	16

Exhibits

Location Map	E-1
ADT Map	E-2
Bridge Structure Map	E-3
Structure Master Report (S107) & BCR Approval Letter	E-4

Exhibits (cont'd)

Design Criteria..... E-5
Cost Estimate E-6
Table of Required Right-of-Way and Easements..... E-7
ESA Memorandum Summary..... E-8
Clearances of Environmental Resources E-9
Categorical Exclusion (BDE 3108) E-10
Lighting Basis of Design E-11

Appendices

Appendix A Utility Coordination
Appendix B FHWA/IDOT Coordination
Appendix C Public Meeting Documentation
Appendix D Project Coordination

Contents – Volume 2

Aerial Plan of the Proposed Englewood Line
Typical Sections
Trail and Access Ramp Profiles
Drainage
Retaining Walls
Detour and Alternate Route Map
Lighting

Separate Volumes

Volume 3 Location Drainage Study
Volume 4 Bridge Inspection Report (MI)
Volume 5 Environmental Survey Assessment – Phase I
Volume 6 Environmental Survey Assessment – Phase II



Local Project Development Report for Group II Categorical Exclusions and Design Approval

County: Cook

Local Public Agency: Chicago Department of Transportation

Section Number: 16-E5482-00BT

Route:

Project Number: CD # E-5-487

Project Length: 1.75 miles

Street/Road Name: Englewood Elevated Railroad Connector (59th Street)

Termini: Hoyne Avenue (West) to Wallace Avenue (East)

For Township or Road District bridge projects: The County Engineer certifies that the project design speed exceeds the minimum design speed recommended for this classification of roadway as provided in the BLRS Manual in order to prevent a deficient NBIS rating for approach roadway alignment appraisal. All elements have been designed to the chosen design speed unless noted otherwise in Section 2(e) and/or the attached BLR 22120.

County Engineer Date

Categorical Exclusion and Design Approval Recommended

Local Agency Date

Regional Engineer Date

This project will not have any significant impacts on the human environment; therefore, the FHWA approves the project as a Categorical Exclusion on Date

Design Approval Bureau of Local Roads & Streets Date

1. LOCATION AND EXISTING CONDITIONS

a. **Location** (*attach location map to supplement narrative description*)

The proposed Englewood Line multi-use trail is a planned conversion of an abandoned rail line into a multi-use trail. The existing abandoned rail line was elevated from street grade in 1917 by the Pennsylvania Railroad. The proposed trail is oriented in the east-west direction, parallel to and in between 58th and 59th Streets, from Hoyne Avenue on the west to Wallace Avenue on the east. The project's right-of-way is being transferred by Norfolk Southern Corporation (NS) to the City of Chicago as part of a land exchange agreement independent of this project.

Refer to **Exhibit 1** for the location map.

b. **Description of Existing Facility** - *Give narrative description, including such items as width of travel, parking and turn lanes, sidewalks, alignment, traffic control devices, utilities, jurisdiction, maintenance responsibility, drainage, terrain and current land use (including major public facilities and local landmarks). Attach existing typical sections showing roadway widths, bridge widths, ROW widths, sidewalk widths, guardrail, curb and gutter and surface types.*

The existing infrastructure is an abandoned elevated railway corridor oriented in the east-west direction. The railway built the rail line in the late 19th century and elevated it in the early 20th century. The line provided passenger and industrial east-west rail connectivity for industry located along the rail corridor. Trains have not operated over the railway since the late 1950's/early 1960's. All large industries who were serviced by the rail line have either closed or relocated away from the area.

The width of the portion of the abandon railway corridor to be used for the proposed trail varies from approximately 22' to 30' along the length of the project. The proposed paved trail will have a width of 12' and have a cross slope of 1% draining from the north edge of pavement to the south edge of pavement. Along each side of the trail pavement will be a 5' wide clear area that will have a max slope of 6:1. The clear area will be grass.

The existing railway has limited access because of steep embankments. The railway is overgrown with vegetation with no visible hard surface capable of supporting vehicles. The railway embankment is steep ranging from 3:1 to 2:1 for the majority of its length. The embankments are maintained by the growth of mature trees, existing retaining walls, and the substructure of old industry lead tracks, whose superstructures have previously been removed.

Refer to **Volume 2** for the existing and proposed typical section of the Englewood Line.

When the railway was elevated in 1917 by the Pennsylvania Railroad, 26 bridge structures were built to grade separate the railway above the streets. The structure numbers are sequential from 016-6352 at Damen Avenue to 016-6377 at Lowe Avenue. Twenty-two of the bridges, Green Street to Damen Avenue (SN 016-6352 to SN 016-6373), are four-span concrete structures with the exception of the three-span structure at Loomis Boulevard (SN 016-6362). These bridges measure approximately 32 feet wide by 70 feet in length and provide a vertical clearance varying from 11'-2" to 13'-6". Each bridge structure spans over two lanes of vehicular traffic. The concrete bridge decks consist of 2 feet of railroad ballast, separated by a 2-inch waterproofing membrane, on top of a 2 feet concrete deck. Four of the bridges, Halsted Street to Union Street, are single span built-up riveted steel through plate girder superstructures with built-up riveted steel through decks. The approximate width of the steel bridges is 13'-0" center-to-center of girders.

All 26 of the existing streets crossing under the abandoned railway are maintained by the Chicago Department of Transportation (CDOT). Two of the existing streets (Ashland Avenue and Morgan Street) are under Cook County Department of Transportation and Highways (CCDTH) jurisdiction.

There is no existing signage, traffic control devices or lighting installed on the railway. There is no drainage system along the railway, except for drainage systems at the bridge structures. Each bridge structure's deck was built with a waterproofing membrane. Deck cores were taken to verify the existence of the waterproofing membrane. The bridge decks are graded to drain towards both the east and west abutments. As-built drawings indicate an existing drainage system drains the water from the bridge abutments into the City sewer system running under the roadways.

The property is currently owned by the NS, but will be transferred over to the City of Chicago as part of a land exchange agreement anticipated to be completed in summer of 2017. The original railway ROW was approximately 60 feet in width; however, the controlling rail entity acquired multiple parcels of land adjacent to the railway ROW. The width of land being transferred to the City and to be converted into a multi-use path varies from 60 feet wide to 150 feet wide within the project limits.

The project is bordered to the north by residential single family homes, planned residential development of the former Chicago Public School (Bon temps) property, vacant lots, vacant buildings, private business, and urban agriculture (Wood Street Urban Farm Growing Home, Inc.); to the east by a car junkyard (Englewood Used Auto Parts) and Mazalene Roberts Towner Scholarship Foundation Cultural Center; to the south by vacant lots, vacant vegetated land, vacant buildings, commercial development, a car junkyard (Robbins Auto Salvage Inc.), the Department of Human Services, Hermitage Park, and urban agriculture (Wood Street Urban Farm Growing Home, Inc.); and to the west by CSX's 59th Street Intermodal Yard.

c. Traffic Data

Current ADT: N/A (proposed trail) % trucks: N/A

Will 80,000 trucks be legally permitted on this route? Yes No

Design Year: 2040 ADT 1,000 users DHV: 300 users % trucks: N/A

2006 ADT Summary		
Source : City of Chicago		
	EB	WB
59 th St. (West of Ashland Ave.)	5,900	5,800
59 th St. (Ashland Ave. to Halsted St.)	8,200	8,100
59 th St. (East of Halsted St.)	6,900	7,800
	NB	SB
Damen Ave.	7,600	7,300
Ashland Ave.	11,800	11,700
Loomis St.	1,700	1,800
Racine Ave.	5,300	5,700
Morgan St.	2,500	2,000
Halsted St.	6,900	6,300

Refer to **Exhibit 2** for the ADT map.

- d. **Structures** - *Identify location within the proposed improvement of all structures on attached location map. Attach a copy of the Structure Master Report for all structures within the project limits. Attach a copy of the Bridge Condition Report or the Bridge Deck Resurfacing approval letter for structures to be replaced, rehabilitated, or resurfaced.*

There are a total of 26 bridges supporting the existing abandoned railway over roadways within the project limits. Twenty-two of the bridge structures are four simple spans of flat slab reinforced concrete superstructure supported by two reinforced concrete full height retaining abutments and three reinforced concrete multi-column piers on pile foundations. Four of the bridge structures are built-up riveted steel through plate girder superstructure with built-up riveted steel trough deck supported by two reinforced concrete full height retaining abutments.

The following table is a summary of the bridge structures within the project limits:

Street Crossing	Structure Number
S. Damen Ave.	016-6352
S. Winchester Ave.	016-6353

S. Wolcott Ave.	016-6354
S. Honore St.	016-6355
S. Wood St.	016-6356
S. Paulina St.	016-6357
S. Ashland Ave.	016-6358
S. Justine St.	016-6359
S. Laflin St.	016-6360
S. Bishop St.	016-6361
S. Loomis Blvd.	016-6362
S. Ada St.	016-6363
S. Throop St.	016-6364
S. Elizabeth St.	016-6365
S. Racine Ave.	016-6366
S. May St.	016-6367
S. Aberdeen St.	016-6368
S. Carpenter St.	016-6369
S. Morgan St.	016-6370
S. Sangamon St.	016-6371
S. Peoria St.	016-6372
S. Green St.	016-6373
S. Halsted St.	016-6374
S. Emerald Ave.	016-6375
S. Union Ave.	016-6376
S. Lowe Ave.	016-6377

Refer to **Volume 4** for the Master Structure Reports (S107) for all 26 bridge structures within the project limits.

Refer to **Exhibit 3** for map showing the location of the bridge structures.

Refer to **Exhibit 4** for a copy of the Bridge Condition Report approval letters for structures to be rehabilitated.

There are a total of 8 existing retaining walls greater than 7 feet of retained height within the project limits. The 8 existing retaining wall are at the following locations along the Englewood Elevated Rail Line:

1. Northeast quadrant at Racine Avenue
2. Northeast quadrant at May Street
3. Southeast quadrant at Pauline Street and Southwest quadrant at Ashland Avenue
4. Southwest quadrant at Wood Street
5. Northwest quadrant at Wood Street
6. Southwest quadrant at Throop Street
7. Northwest quadrant at Throop Street
8. Southwest quadrant at Ada Street

There are seventeen (17) proposed retaining walls for the Englewood Trail corridor improvements. The retaining walls are proposed to construct the access ramps to be ADA compliant while limiting the impacts to the existing mainline embankment and minimizing the need for additional right-of-way.

Fourteen (14) of the proposed retaining walls have a retained height of less than seven feet. For these retaining walls, a segmental block wall is being proposed. A segmental block wall has an estimated price of \$75 per square foot.

Three (3) of the proposed retaining walls are required to have a retained embankment height greater than 7'. The proposed retaining walls with a retained height greater than 7' will be a permanent sheet pile wall with a concrete cap and a bicycle railing. These three walls are located at the Hermitage Ave, Racine Ave, and Morgan St access ramps. The sheet pile walls have an estimated price of \$125 per square foot of exposed area. Type, Size, and Location drawings have been generated for the three permanent sheet pile retaining walls and are included in **Volume 2**.

Refer to **Volume 2**, Aerial Plan of the Proposed Englewood Line sheets for exhibit location of the existing and proposed retaining wall structures.

e. **Railroads** - Identify location of all railroad crossings on attached location map and complete the following:

Railroad Name	No. and Type of Tracks (Main or Switching)	Type of Warning Devices*	No. of Trains Per Day	Railroad Width of Crossing at Rt. Angles
N/A				

*Include a sketch showing location of railroad protective devices from the edge of roadway and to the nearest track.

- f. **Contiguous Sections** - Describe the existing typical sections at each end of the proposed improvement including number of travel lanes, turning lanes and parking lanes, lane widths and roadway width (f-f of curbs or e-e of shoulders), and sidewalk width.

The west terminus of the trail will terminate at-grade prior to reaching the existing 5 foot wide sidewalk along the eastern side of Hoyne Avenue between 58th St. and 59th St. The proposed trail profile will match the existing abandon railway profile and descend from being elevated at Damen Avenue to being at-grade prior to reaching Hoyne Avenue. The proposed trail will not continue west beyond Hoyne Avenue. The trail width will be 12 feet and terminate at the perpendicular intersection of the existing sidewalk.

The east terminus of the trail will terminate at-grade upon intersecting the eastern 5 foot wide sidewalk of Lowe Avenue. The trail terminus at Lowe Avenue will be in the northeast quadrant of the proposed trail crossing over Lowe Avenue. The proposed trail width will be 8 feet. The proposed trail will not extend east beyond the project terminus.

2. Proposed Improvement

- a. *Discuss the purpose and need of the project.*

The need for the Englewood Trail has been identified in multiple pasts plans, including the *Health Impact Assessment of the Proposed Englewood Line Trail (HIA)* published in September 2016 by the Chicago Department of Public Health, the *Green Healthy Neighborhoods Plan* published in 2014, the *New ERA Trail Community Vision Plan* published in August 2009, and *The Englewood: Making a Difference Quality of Life Plan* published in December 2005.

The vision for the Englewood Line multi-use path as stated in the *New ERA Trail Community Vision Plan* is to be a “highly visible and highly tangible evidence that the transformation of Englewood is underway and here to stay.”

The Englewood Trail will improve the existing infrastructure and enhance the aesthetics of the existing railroad land. The improvements will create the infrastructure to support new residential and commercial investment adjacent to the path for the path users to support. The path will encourage the continued development of an urban agriculture district in the area, which is generating jobs in the area, by fostering stability to the neighborhood and creating a signature destination within the area.

The Englewood Trail will enhance community safety. The project will create additional active spaces and encourage collaboration with schools, community organizations and economic development efforts to enhance safety. The planned continued development of the urban agriculture within the corridor, the residential and commercial redevelopment anticipated adjacent to the trail, and the elimination of the isolating railroad barrier will attract a greater presence of residents and businesses to the area replacing the vacant land and buildings that currently exist. The path’s conversion of unused, isolated, vacant space into a single, connected, open space that is well lit and maintained will eliminate some of the existing higher crime areas. The HIA cited “multi-risk comprised of characteristics in both the social and built environment such as vacant structures, guns, gangs, drugs, poor lighting and isolation“ as a direct link to crime-related safety concerns. The reduction in vacant buildings, poor lighting and isolation will help to reduce some of the components generating crime.

The Englewood Trail will connect the neighborhood by converting the unused railroad right-of-way that currently serves as a barrier separating the adjacent properties on the north and south sides of the railroad land. The proposed project will provide access to the path from both the north and south at reasonably located intervals to ensure equal access for the entire neighborhood. The path will be tied into the City’s existing bicycle system and allow for the additional expansion of the City’s DIVVY program in the area. This will bring access to more modes of transportation to the residents. It will also provide connectivity to Lindblom School and Park, Hermitage Park and Nichololson Academy.

The Englewood Trail will provide a means to improve the overall health of the neighborhood residents. As cited in the HIA, “the communities within a mile of the proposed path have multiple health indicators that are significantly higher than the City overall, indicating that children and adults are suffering from acute effects of chronic disease in the area.” Construction of the path and access ramps will provide increased access to green space for the neighborhood residents that has traditionally led to an

increase in physical activity for residents. An increase in residents' overall physical activity will result in associated health benefits. The path increases access to green/open space for residents which is anticipated to improve the mental health status of the residents by exposing them to greater tree cover and reducing the sense of urban isolation.

b. What design guidelines will be used for the proposed improvement? (Check One)

- Rural (BLRS Manual Chapter 32)
- Urban (BLRS Manual Chapter 32)
- Suburban (BLRS Manual Chapter 32)
- 3R Guidelines (BLRS Manual Chapter 33)
- Bicycle Guidelines (BLRS Manual Chapter 42)
- Pedestrian Guidelines
- Other: Refer to **Exhibit 5** for design criteria approved by the City of Chicago.

Functional Classification: Arterial Collector Local Road Pedestrian/Bikeway
Other _____

Terrain: Level Rolling

Regulatory or Posted N/A Design Speed: 12 mph
Speed Limit: _____

c. *Describe type of work to be accomplished by the improvement. Discussion should include width of proposed travel, parking, bicycle and turning lanes, sidewalks, shared-use paths, guardrail, traffic control devices, drainage items (including storm sewer outfalls), alignment changes, railroad work, utility adjustments, intersection improvements, side slopes and clear zones. Specify the e_{max} for horizontal curves. Attach typical sections, plan and profile sheets, and intersection design studies when applicable.*

The proposed project is the conversion of approximately 1.75 miles of an abandoned elevated rail line to a multi-use path. Refer to **Volume 2** for a plan view aerial of the proposed Englewood Line. The proposed trail will maintain the existing vertical profile of the railroad property.

The project does not include improvements to the 26 roadways crossing under the structures supporting the elevated path. Reconstruction of existing sidewalks that are in poor condition and connect to the access ramps for the path will be limited to less than one block in length and will not extend beyond 58th Street or 59th Street. Any sidewalk reconstruction will replace the sidewalk in-kind in terms of location and width.

The proposed project will not impact existing vehicular traffic patterns on the adjacent surface streets. There are no planned changes to vehicular traffic patterns, signalized intersections, roadway alignments, parking configurations or the location of existing pedestrian crossings.

Refer to **Volume 2** for proposed typical sections. The proposed path will be a 12 foot bituminous surface with an adjacent 5 foot wide grass area on each side of the trail with a slope less than 6:1. The proposed project will minimize impacts to the existing embankments where possible in order to maintain the existing trees growing on the embankment.

There are a total of 11 proposed access ramps along the entire length of the elevated trail. The two primary access trails, located at Hoyne Avenue and Halsted Street, are 12 feet wide and will serve as access points for emergency service vehicles and police vehicles to access the trail. All other proposed trail access ramps are 10 feet wide. The access ramps will be constructed with bituminous pavement matching the mainline trail pavement section.

The project will install 8" of CA-7 under the proposed paved trail and 5' wide clear zones for a total width of 22' of CA-7. The CA-7 will provide detention for stormwater during rain events. New underdrains will be provided to capture stormwater runoff at the access ramp areas and route it to designated detention systems. The detention systems will be comprised on areas of underground CA-7. A drainage structure with a restrictor will be used to connect to existing City storm sewers for

areas at and adjacent to the existing bridge structures.. Refer to **Volume 3** for the Location Drainage Study.

The existing 26 bridges are intended to be rehabilitated per the recommendations contained in the approved BCRs. Refer to **Exhibit 4** for the IDOT BCR approval letters.

A total of 17 retaining walls will be rehabilitated or installed. Refer to **Volume 2** for Retaining Walls.

- d. *Discuss items affecting improvement such as hazardous mailbox supports, parking and truck restrictions, mail delivery from traffic lanes, justification (including warrants) for multi-way stop signs, traffic signals and other traffic control and railroad protective devices, stage construction, nearby airports, and additional lighting:*

Trail specific lighting will be installed along the trail and access ramps. The proposed lighting work shall consist of furnishing and installing an inground pole mounted downlight fixture, Model # RX132-G2-2NA3-15, as manufactured by Philips Industries. The fixture will be complete with a cobra head style area fixture made of die-cast aluminum housing with clear glass lens LED, lamp. The lighting poles will be steel with a curved shape to give more space at the bike path level as shown on the detail included on the lighting plans included in **Volume 2**.

The trail lighting network will be controlled by six lighting controllers and fed from ComEd 240V service. The lighting will be metered.

The Illuminating Engineering Society of North America (IESNA) Lighting Handbook Tenth Edition (IES-RP-33-14, Table-3), Mixed use area (pedestrian ways and bike ways) recommended illuminance target is minimum 25 lux (2.5 fc), maximum 65 lux (6.5 fc). The City of Chicago Department of Transportation (CDOT) Division of Engineering's Design Requirements and Guidelines (dated September 18th, 2015) requires a minimum light level that is 50% greater the IESNA requirement. This results in the minimum illuminance required equaling 25lux + 12.5lux = 37.5 lux (3.75fc) for the lighting design. This represents the current recommended practice for outdoor lighting and typical applications found in the City of Chicago.

Per the typical condition photometric calculations performed and the proposed spacing of the luminaires, the City's minimum illumination level in most areas of proposed pavement for the project is satisfied. There are a small percentage of areas where the maximum illumination level is exceeded and a few areas where the minimum illumination falls short of 3.75 fc. This can be expected with a lighting design of this type. The proposed lighting design meets the intent of the City lighting requirements. Refer to **Exhibit 11** for lighting basis of design.

- e. *Identify each aspect to be constructed at less than the design guidelines and provide a clear description of required design variances and appropriate justification. (BLRS Manual Section 27-7). If a design variance is required, include a copy of the approved BLR 22 120 form as an attachment.*

The proposed trail has a total of 26 existing bridges. The existing vertical clearance for all of these bridges range from 11'8" to 13'-10", which is below the minimum 14'-9" required for an existing bridge over a two lane local roadway (BLRS 36-1, Figure 36-4I).

The Level II Design Criteria Checklist BDE 3108 Form is included as **Exhibit 10**.

- f. Current estimated cost of proposed improvement? \$ 44,262,785

Refer to **Exhibit 6** for Opinion of Probable Construction Cost.

- g. *Analyze the need for accommodating pedestrians, bicyclists and the handicapped. When applicable, describe the facilities to be provided for pedestrians and bicyclists. Discuss the ADA accessibility and maximum longitudinal grade of these facilities. (BLRS Manual Chapter 41)*

The Englewood Trail is intended to serve pedestrians and bicyclists only. The only vehicular access to the trail will be at the eastern and western termini and be for emergency service, maintenance or

police vehicles only. The mainline trail and all access ramps will maintain a longitudinal slope less than 5%. There will be no stair accesses along the path. Sidewalks/Shared-Use Paths:

Maximum 2% cross slope: Yes No Not Applicable

ADA ramps with detectable warnings at street intersections:

Yes No Not Applicable

If no, provide justification.

- h. *Discuss any proposed improvements being considered in adjacent segments including the anticipated construction startup date of these improvements.*

There are no known proposed public improvements anticipated within the corridor.

Currently the former Bon Temps School (CPS) and adjacent playground are under contract for a planned private development for residential structure.

Growing Englewood (Urban Agriculture), private business, is anticipating further expansion on to the vacant lots adjacent their site to increase their farming footprint.

3. **Crash Analysis (BLRS Manual Section 22-2.11(b)(9))**

- a. *Summarize crash data for the past five years, including a spot map or a location map showing crash locations when possible. Detail the types of crashes and include collision diagrams, if possible, especially at cluster sites. Give the source of this data.*

The Englewood Trail Study did not include crash or traffic analysis. The existing traffic data (year 2006) was obtained from City of Chicago's online data portal. Refer to **Exhibit 2** for ADT exhibit.

- b. *Analyze available crash data including results of field check. Discussion should include high crash locations, critical wet weather sites, and other crash patterns. If the data is inconclusive, make a statement to that effect.*

Not applicable

- c. *Describe how the proposed project will address any crash issues.*

Not applicable

4. **Right-of-Way**

- a. *Describe the right-of-way taking, including the total acreage required for each of the following categories: ROW, permanent easements, temporary easements and temporary land use permits. Include the width of taking, number of property owners, acreage of right-of-way and/or easements, character of land; i.e., farm, residential, commercial or publicly owned properties, anticipated impacts to properties that remain, and location of any improvements with respect to required right-of-way. Discuss any impacts on setbacks required by zoning.*

The main trail will be located on City of Chicago property obtained from Norfolk Shore Rail Road as part of a land exchange agreement independent of this project.

The proposed access ramps at three locations are anticipated to require the acquisition of additional ROW in order to be constructed and maintained. The locations requiring additional ROW are:

1. NE corner of Hoyne Avenue and Englewood Trail. Approximately 0.34 Acres on 1 parcels will be acquired at this location. The property is currently unused, concrete pavement. Refer to **Exhibit 7** for parcel(s) information.
2. SE corner of Damen Avenue and Englewood Trail. Approximately 0.28 Acres on 4 parcels will be acquired at this location. The property does not contain any buildings. There is a billboard on one of the parcels at this location. Refer to **Exhibit 7** for parcel(s) information.
3. SE corner of Ashland Avenue and Englewood Trail. Approximately 0.82 Acres on 9 parcels will be acquired at this location. There is a single building located on the parcels and parking lots associated with the building. Refer to **Exhibit 7** for parcel(s) information.

Refer to **Exhibit 7** for required right-of-way to be acquired.

- b. Are any residents, businesses or farms to be displaced?

Yes No

If yes, describe the number and type of displacements anticipated and mitigation that will be taken to provide relief for this impact on an attached sheet.

The property to be acquired along Ashland Avenue is not an existing business or residence. However, the building is currently vacant and for sale. The building has the potential to be used for a business.

5. Prime Farmland (BLRS Manual Section 20-10)

- a. If the project requires more than 3 acres/mile (0.75 hectares/kilometers), 10 acres (4 hectares) for a non-linear improvement, or the project ROW is not contiguous to the existing ROW, contact the Illinois Department of Agriculture and attach results of the coordination and summarize the results below.

Not applicable

- b. The project requires consultation with the Natural Resource Conservation Service., Form AD-1006 has been completed and submitted to the local office of NRCS. The completed AD-1006 form is attached.
- The impact of this project on farmland conversion has been evaluated in accordance with the requirements of the US Natural Resources (NRCS). The project will cover 3 acres or less of farmland per mile (0.75 hectares or less of farmland per kilometer) and the conversion will not result in more than minor impacts. Accordingly, the project conforms to the general form AD-1006 prepared by NRCS. Therefore, further coordination with NRCS on this project will not be necessary.

Not applicable

6. Floodplain Encroachment (BLRS Manual Section 20-7)

Does the proposed work cross or encroach upon a 100-year floodplain, including a regulatory floodway?

Yes No

If yes, summarize the location hydraulics study, regulatory floodway restrictions, the effect of any encroachment (including a comparison between existing and proposed conditions) and the effect of over-the-road flow on the proposed transportation facility. Attach any available floodplain maps.

7. Phase I & II NPDES Storm Water Permit Requirements (BLRS Manual Section 7-4.01)

Will the project involve soil disturbance of 1 acre (0.4 hectares) or more?

Yes No

If yes, the project must comply with the Phase II NPDES Storm Water Permit Requirements.

8. **“404” Permit (BLRS Manual Section 7-4.02)**

Does this project involve waters regulated by Section 404?

Yes No

If yes, what type of 404 permit is required? Nationwide Individual Regional None

Attach a copy of any 404 permit authorization and/or coordination letters with the Corps of Engineers.

If an individual Section 404 permit is required, please notify the Illinois Department of Transportation district office before submitting the application.

9. **Special Waste (BLRS Manual Section 20-12)**

a. Following the special waste assessment screening criteria shown on Figure 20-12A of the BLRS Manual, is Preliminary Environmental Site Assessment (PESA) required?

Yes No

b. Is work being done on property in the name of the state or are contract plans being prepared by the state?

Yes No

c. If a PESA is required for either state or local ROW, did the PESA results determine that the project has Recognized Environmental Conditions (REC's) for special waste?

Yes No

If the PESA results determine that the project contains REC's, describe how the special waste is proposed to be handled (including if a Preliminary Site Investigation (PSI) is required).

The City of Chicago (the City), Department of Fleet and Facility Management contracted Tetra Tech Inc. (Tetra Tech) to perform Phase I and Phase II Environmental Site Assessments (**See Volume 5 and 6**) of approximately 1.92 miles of an elevated railroad embankment in the Englewood neighborhood of Chicago, Cook County. The studies were performed in 2015, and were designed to identify and characterize the environmental conditions at the Site, and estimate a range of environmental costs associated with addressing these conditions during a proposed construction project in the area.

Thirty (30) boring locations were associated with the Phase II, and nearly 70 soil samples were submitted for laboratory analysis. Approximately 20 constituents of concern (COC) were identified at the subject property exceeding Illinois screening standards, primarily polynuclear aromatic hydrocarbons (PAHs) and metals, very typical chemical constituents found in historically industrial areas.

Patrick's review of the resulting data indicates that the contaminants appear to be randomly distributed across the various soil-boring locations; most boring locations contained at least one COC that exceeded the screening standard for at least one exposure pathway. Such a random distribution is commonly found in historically industrial areas; there do not appear to be "hot spots" associated with any particular historical activity or spill event. As such, a focused remedial approach, based upon a limited excavation of specific areas, does not appear practical here. On the other hand, removal of all of the impacted soils (as estimated on the basis of somewhat limited data) would be excessive and impractical (over 40,000 tons of soil, costing in excess of \$2.5 million).

In the Phase II report, Tetra Tech suggests three additional alternatives to the complete removal approach described above. The second of these alternatives would remove only the top three feet of impacted soils, and replace them with clean fill for the entire width of the 30-foot-wide elevated railway. Lead-impacted soils which are considered hazardous (based upon the results of a leachability test) would be chemically stabilized regardless of depth, and left in place. This technical approach is explicitly allowed under the Illinois EPA's Site Remediation Program (SRP), and would likely be successful and acceptable to the Illinois regulators. This approach was estimated by Tetra Tech to cost \$582k.

A third approach suggested by Tetra Tech would be similar the preceding alternative, but instead of excavating impacted soils to a depth of 3 feet, only 18 inches would be removed and replaced by a geotextile fabric overlain by 18 inches of clean stone (across the entire 30-foot width of the elevated railway). This approach is not explicitly allowed within the regulations governing the Illinois SRP, but such an approach has been approved by Illinois EPA in the past (Patrick has itself had had such projects approved in the recent past). As there is a strong precedent for Illinois EPA approval, this approach is reasonable, and is somewhat less expensive than the second alternative; Tetra Tech estimates a total cost of \$426k.

Tetra Tech also offered a fourth alternative. This last alternative is the same as the preceding approach, except that instead of removing soils across the entire 30-foot width of the railway, only an 8-foot zone would be cleared, along the footpath alignment. This footpath would include some degree of restriction for off-path use (signage, etc.), and would result in a much lower excavation cost. However, Patrick does not anticipate that the Illinois EPA would agree to such an approach. In this case, an engineered barrier (the stone) would be used in conjunction with an institutional control (off-path restrictions) in a way that does not have any known precedent. Illinois EPA is likely to raise the issue that park goers may very well ignore posted signs, and come into contact with contaminated soils, making the institutional control ineffective and unenforceable. For this reason, Patrick regards this alternative as carrying an unacceptable degree of regulatory risk. Patrick recommends the third excavation alternative be implemented. A conservative remediation cost of \$500k may be assumed, with the caveat that it is based on the relatively limited amount of data collected from the Site (only 30 borings installed along a nearly two-mile length of railway).

10. Environmental Survey (BLRS Manual Section 20-2)

Whenever a project involves land acquisition (including easements), any in-stream work (including drainage structure run-around), is located within or adjacent to historic properties listed in (or eligible for) the National Register of Historic Places, a bridge on the historic list, is near wetlands, or known locations of threatened or endangered species, the Environmental Survey Request Form should be submitted early in the project development phase.

- a. Wild and Scenic Rivers - If this project crosses or affects a river on the National Wild and Scenic Rivers System or a river listed in the Nationwide Inventory of Rivers with potential for inclusion on the system, include coordination between the National Park Service and the Bureau of Design and Environment (BDE).

Involvement No Involvement

- b. Wetlands - Does the proposed work impact the use of regulatory wetlands?

Yes No

If yes, indicate how the wetlands will be mitigated. Banking Accumulation On-site Other

- c. Archaeological and Historical Preservation Include results of coordination. Does the project impact an archaeological or historic preservation site?

Yes No

If yes, describe any required documents.

- d. Threatened or Endangered Species – Does the project impact any endangered species or plants?

Involvement No Involvement

Include copy of biological resources memorandum or signoff by BDE and/or IDNR.

- e. Stream Modification and Wildlife Impacts - Include copies of any correspondence between BDE and IDNR or U.S. Fish and Wildlife Service. Attach copies of any additional coordination between local agency and IDNR or U.S. Fish and Wildlife Service whenever required as a result of biological review by BDE. Address any proposed mitigation measures.

Involvement No Involvement

11. Section 4(f) Lands (BLRS Manual Section 20-3)

a. Does this project require any right-of-way, including temporary construction easements, from a publicly owned park, recreational area, wildlife and waterfowl, or any historic site in or eligible for the National Register of Historic Places?

Yes No

b. If yes, what type of the Section 4(f) involvement has been completed?

Section 4(f) de minimis Standard Section 4(f) Temporary Occupancy None

12. **Air Quality (BLRS Manual Section 20-11)** Check One:

a. This project is in an attainment area.

Projects within a portion of a nonattainment area for which the Chicago Metropolitan Agency for Planning (CMAP) is the MPO.

This project is included in the Go To 2040 (transportation plan) and in the Transportation Improvement Program (TIP), endorsed by the CMAP, the region's Metropolitan Planning Organization. The Go To 2040 (transportation plan) was found to conform by the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) on _____.

The TIP was found to conform by FHWA on _____ and by FTA on _____.

Projects within a nonattainment area served by a Metropolitan Planning Organization other than CMAP.

This project is included in the Long-Range Transportation Plan and in the _____ Transportation Improvement Program (TIP) endorsed by _____, the Metropolitan Planning Organization (MPO) for the region in which the project is located.

On _____ the Federal Highway Administration (FHWA) and the Federal Transit Administration (FTA) determined that the Long-Range Transportation Plan conforms with the transportation-related provisions of the Clean Air Act Amendments of 1990. The FHWA and the FTA determined on _____ that the TIP conforms with the Clean Air Act Amendments. These findings were in accordance with 40 CFR Part 93, "Criteria and Procedures for Determining Conformity to State or Federal Implementation Plans of Transportation Plans, Programs, and projects Funded or Approved Under Title 23 USC or the Federal Transit Act."

The project's design concept and scope are consistent with the project information used for the TIP conformity analysis. Therefore, this project conforms to the existing State Implementation Plan and the transportation-related requirements of the 1990 Clean Air Act Amendments.

Not applicable per BLRS Manual 20-11.03(c)2 Air Quality. Bicycle and pedestrian facility projects are exempt.

b. **Mobile Source Air Toxics (See BDE PM 52-06)**

This project will not result in any meaningful changes in traffic volumes, vehicle mix, location of the existing facility, or any other factor that would cause an increase in emissions relative to the no-build alternative. As such, FHWA has determined that this project will generate minimal air quality impacts for Clean Air Act criteria pollutants and has not been linked with any special Mobile Source Air Toxic concerns. Consequently, this effort is exempt from analysis for MSATs.

Moreover, EPA regulations for vehicle engines and fuels will cause overall MSATs to decline significantly over the next 20 years. Even after accounting for a 64 percent increase in VMT, FHWA predicts MSATs will decline in the range of 57 to 87 percent, from 2000 to 2020, based on regulations now in effect, even with a projected 64 percent increase in VMT. This will both reduce the background level of MSATs as well as the possibility of even minor MSAT emissions from this project.

c. Construction-related Particulate Matter

Demolition and construction activities can result in short-term increases in fugitive dust and equipment-related particulate emissions in and around the project area. (Equipment-related particulate emissions are usually insignificant when equipment is well maintained.) The potential air quality impacts will be short-term, occurring only when demolition and construction work is in progress and local conditions are appropriate.

The potential for fugitive dust emissions typically is associated with building demolition, ground clearing, site preparation, grading, stockpiling of materials, on-site movement of equipment, and transportation of materials. The potential is greatest during dry periods, periods of intense construction activity, and during high wind conditions.

The Department's *Standard Specifications for Road and Bridge Construction* include provisions on dust control. Under these provisions, dust and airborne dirt generated by construction activities will be controlled through dust control procedures or a specific dust control plan, when warranted. The contractor and the Department will meet to review the nature and extent of dust-generating activities and will cooperatively develop specific types of control techniques appropriate to the specific situation. Techniques that may warrant consideration include measures such as minimizing track-out of soil onto nearby publicly-traveled roads, reducing speed on unpaved roads, covering haul vehicles, and applying chemical dust suppressants or water to exposed surfaces, particularly those on which construction vehicles travel. With the application of appropriate measures to limit dust emissions during construction, this project will not cause any significant, short-term particulate matter air quality impacts.

d. Project-level Hot Spot Analysis. Check One:

- This project is in an attainment area and does not require a hot spot analysis.
- This project does not meet the definition of a project of air quality concern as defined in 40 CFR 93.123(b)(1).
Due to Classification of bikeway
it has been determined that the project will not cause or contribute to any new localized PM2.5 or PM10 violations or increase the frequency or severity of any PM2.5 or PM10 violations. USEPA has determined that such projects meet the Clean Air Act's requirements without any further Hot-Spot analysis.
- This project is in a non-attainment or maintenance area and is a project of air quality concern. Therefore, a qualitative hot spot analysis is required. See Attachment _____.

e. COSIM

Are through lanes or auxiliary turn lanes being added with this project?

- Yes No

If yes, has a COSIM pre-screen analysis been completed?

- Yes No

If yes, pre-screen analysis is attached as Attachment N/A.

If no, explain why an analysis has not been performed. N/A

If yes, did the COSIM pre-screen analysis pass or fail? Pass Fail

If the COSIM pre-screen analysis failed, a full COSIM analysis would be required.

13. Noise (BLRS Manual Section 20-6)

The referenced project meets the criteria for a Type III project established in 23 CFR Part 772. Therefore, the proposed project requires no traffic noise analysis or abatement evaluation. Type III projects do not involve added capacity, construction of new through lanes, changes in the horizontal or vertical alignment of the roadway, or exposure of noise sensitive land uses to a new or existing highway noise source.

Based on the traffic noise analysis and noise abatement evaluation conducted, highway traffic noise abatement measures are likely to be implemented based on preliminary design. The noise barriers determined to meet the feasible and reasonable criteria are identified on the attachment. If it subsequently develops during final design that constraints not foreseen in the preliminary design or public input substantially change, the abatement measures may need to be modified or removed from the project plans. A final decision of the installation of the abatement measure(s) will be made upon completion of the project's final design and the public involvement process.

If this project involves a new alignment, additional lanes, or involves a significant alignment change, attach a traffic noise analysis.

The proposed improvement will have no permanent increase to existing noise levels. The bikeway is intended for non-motorized traffic. Equipment used in the construction of the bikeway may produce noise, which affects adjacent properties. To minimize the effects of construction noise, the contractor will be required to adhere to the restrictions stated in the Standard Specifications for Road and Bridge Construction.

14. Work Zone Transportation Management Plans

Does the project intersect or follow a state route?

Yes No

Is the state or local route considered a significant route?

Yes No Not Applicable

If yes, describe how the Work Zone Transportation Management Plan is being implemented.

15. Complete Streets (BLRS Manual Chapter 10)

Does the project include the addition of a travel, turning, or bi-directional turn lane on a state highway?

Yes No

If yes, describe how the Complete Streets Law requiring accommodating bicyclists on a state route apply.

16. Maintenance of Traffic (BLRS Manual Section 22-2.11(b)(9))

Discuss how vehicle traffic and pedestrians will be accommodated during construction, including the impacts of any road and/or sidewalk closure. If the road will be closed, include information concerning location of alternate routes, their ability to handle the additional traffic (street width, number of traffic lanes, structural adequacy, etc.), and the amount of adverse travel. When a marked detour route will be provided, include coordination with appropriate agencies, a description of the adverse travel, and include a map showing the alternate routes or marked detour in the report.

Long term lane, sidewalk, or roadway closure is not anticipated for the construction duration of this project. Short term lane, sidewalk, or roadway closures may be necessary at times during embankment construction and bridge rehabilitation. For single lane closures, traffic will pass by with use of a single lane and the use of a flagger. For roadway closures, traffic will be detoured to adjacent streets via 58th Street or 59th Street. All closures shall conform to IDOT highway standards and specifications. Local traffic will be detoured as required along area streets.

All closures of existing sidewalk would be limited to a single side of a street at a time and would be temporary with durations varying from less than a week (for connections of ramps) to 2-3 months for bridge rehabilitation. At least one side of the impacted streets will be available to pedestrians and/or bicyclists. If applicable, sidewalk closure warning signs will be provided at the nearest intersection to the access ramps.

Refer to **Volume 2** for temporary detour map.

Rehabilitation of the bridge structures is anticipated to be staged so that a single lane of traffic remains open for all residential streets. Traffic would be limited to local traffic only with temporary stop control installed on streets with 2-way traffic to accommodate the single lane under the bridge structure. For the arterial streets (Halstead, Racine, Ashland, Damen), a single lane of traffic in both the north and south direction is anticipated to be maintained during rehabilitation of the bridge structures.

17. Public Involvement (BLRS Manual Chapter 21)

- a. *Summarize public informational meetings, formal public hearings, property owner signoffs, council or board meetings, media coverage, and personal contact with public. Include copies of newspaper advertisements, letter to property owners, public comments, and documents showing all public comments have been addressed.*

As Part of the Chicago Department of Planning and Development's Englewood Line Study, the following public outreach events were held:

1. Trail Outreach – Grow Greater Englewood Community Event, October 3, 2015
2. Trail Outreach – Feed, Clothe, Help the Needy Community Event, January 12, 2016
3. RAGE Village Meeting - May 16, 2016
4. Walking Tours April and May, 2017
5. Trail Outreach – Grow Greater Englewood Walking Tour April 23, 2017

Individual coordination with affected property owners took place over the course of this project. Written correspondence was conducted with Mr. A Haroon, Mr. Syed Razi, Mr. Lee Roy Ball, Mr. John Hanches, and Rev. Vesta L. Dixon. This correspondence is included within **Appendix C**.

In order to introduce the bikeway to a wider audience of potential users, a public informational meeting was held on July 13, 2017. Public meeting documentation is presented as **Appendix C**.

- b. Has any opposition been expressed toward the improvement?

Yes No

If yes, briefly discuss the type and extent of opposition.

- c. If yes, discuss how the opposition has been addressed with the property owners?

18. **Coordination: LA-IDOT-FHWA (BLRS Manual Section 22-1.02)**

Have there been any coordination meetings for this project? Yes No

If yes, list the date(s) of the coordination meeting(s) below and attach coordination meeting minutes in the report.

November 25, 2015; 3:00 PM (Initial Meeting)

January 20, 2016; 1:00 PM – 2:30 PM

June 12, 2017; 1:00 PM – 3:00 PM

19. **Other Coordination**

Chicago Park District. May 8, 2017

20. **Summary of Commitments**

Not applicable

Summary of Attachments (when required):

1. Location Map and Functional Classification Map
2. Existing and Proposed Typical Sections
3. Structure Master Report
4. Bridge Condition Report Approval Cover Letter
5. Preliminary Bridge Design and Hydraulic Report Approval Cover Letter
6. Railroad Crossing Drawing
7. Plan and Profile Sheet (for Rural Projects with additional ROW, Urban Projects, bike trail or sidewalk projects, and Bridge Projects)
8. Intersection Design Studies
9. Spot Map and/or Collision Diagram
10. Soil Conservation Service and Illinois Department of Agriculture Coordination
11. "404" Permit correspondence
12. Environmental Clearances and Correspondence
13. Property Owner Signoffs and/or Correspondence with Property Owners Regarding Public Comments
14. Public Information Meeting Newspaper Advertisement and a Copy of Property Owner Letter
15. Bimonthly Coordination Meeting Minutes
16. BLR 22120 Design Variance Form
17. Detour or Alternate Route Map
18. Other Coordination

EXHIBIT 1

Location Map

Prepared For:

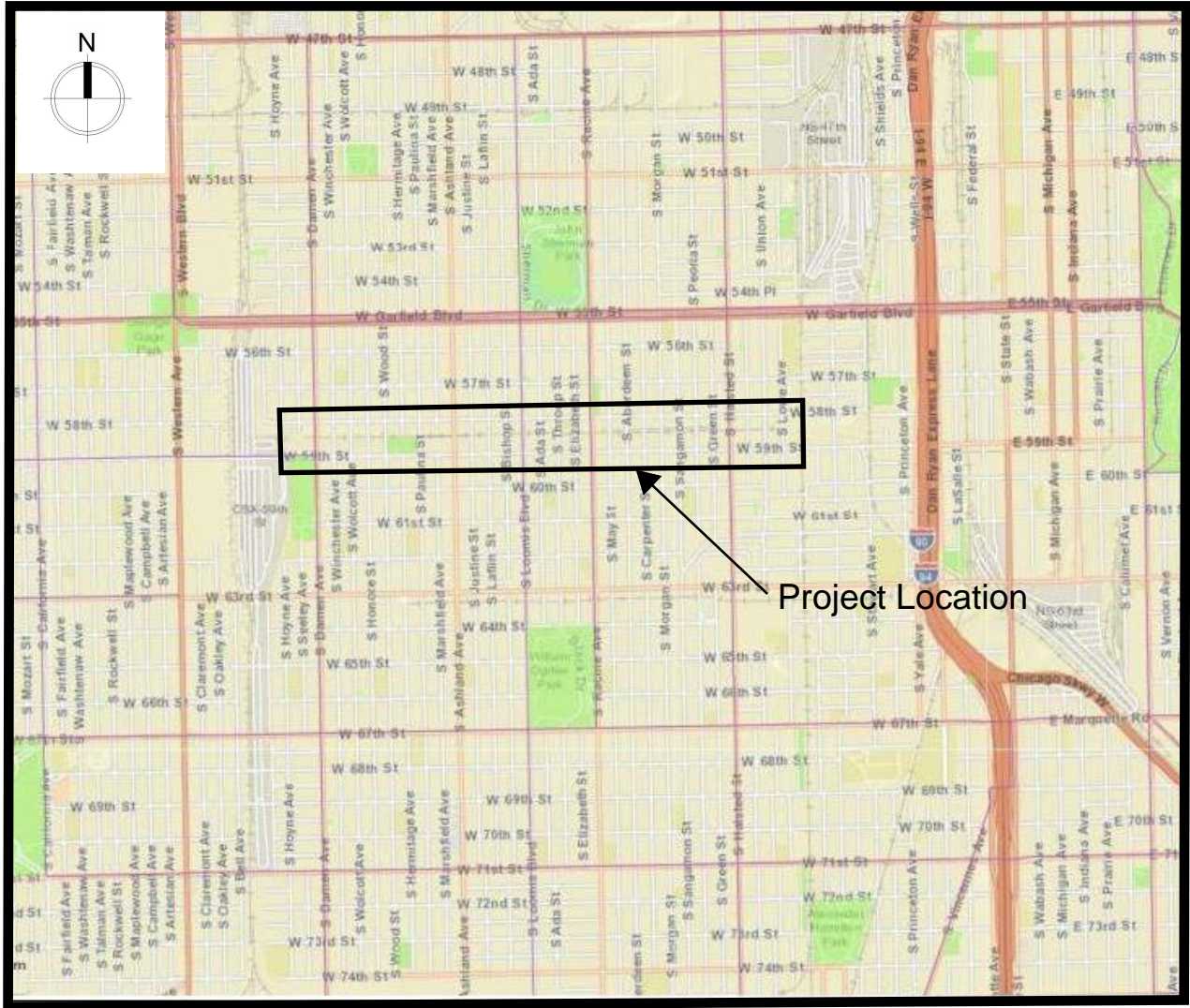
Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



Project Location Map



Project Location

Englewood Trail

Section No. 16-E5482-00-BT
Englewood and West Englewood
Neighborhoods

Chicago, IL
Cook County

EXHIBIT 2

ADT Map

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

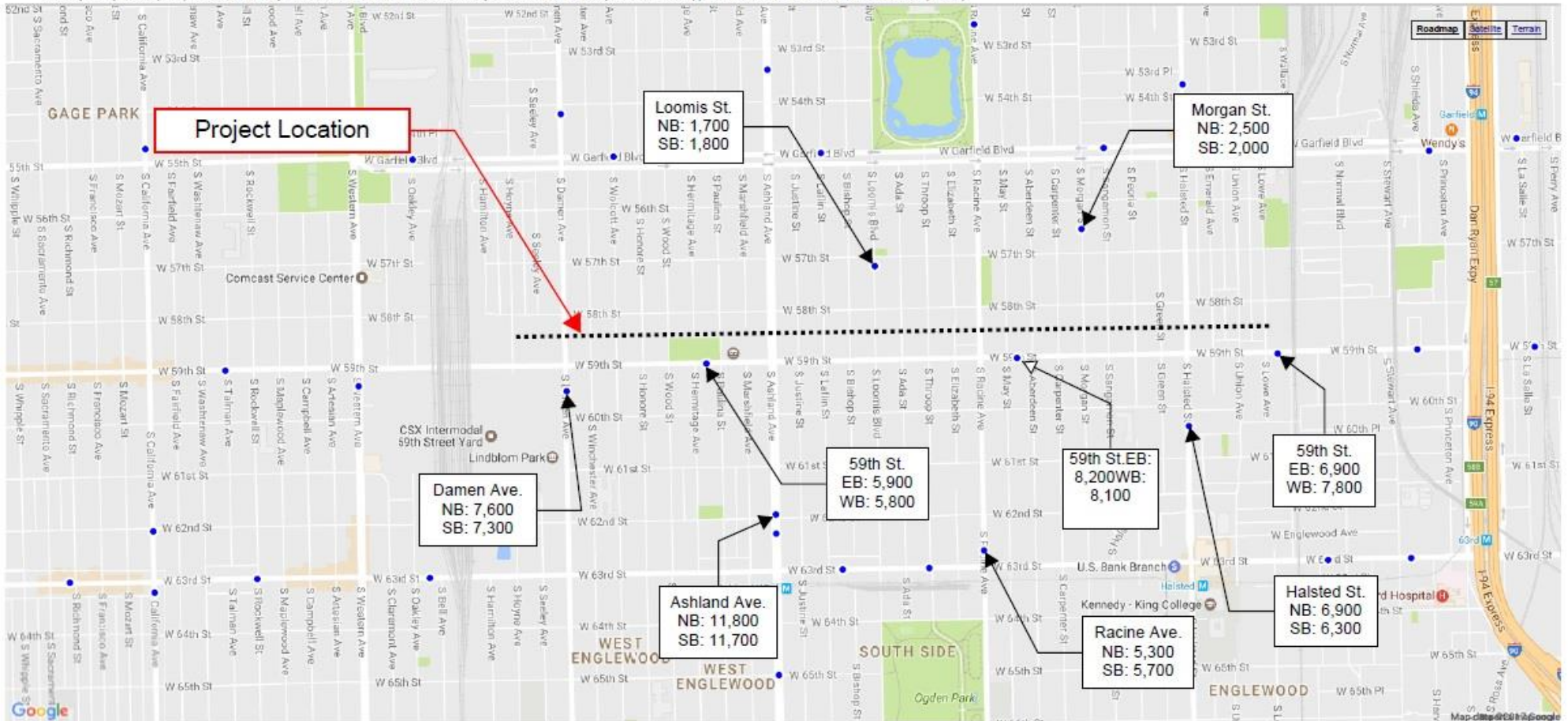
Prepared By:



Average Annual Daily Traffic Data Year: 2006

Based on [Average Daily Traffic Counts - Map](#)

Average Daily Traffic (ADT) counts are analogous to a census count of vehicles on city streets. These counts provide a close approximation to the actual number of vehicles passing



2006 ADT Summary		
Source : City of Chicago		
	EB	WB
59th St. (West of Ashland Ave.)	5,900	5,800
59th St. (Ashland Ave. to Halsted St.)	8,200	8,100
59th St. (East of Halsted St.)	6,900	7,800
	NB	SB
Damen Ave.	7,600	7,300
Ashland Ave.	11,800	11,700
Loomis St.	1,700	1,800
Racine Ave.	5,300	5,700
Morgan St.	2,500	2,000
Halsted St.	6,900	6,300

Information has been provided for streets with proposed points of access to the proposed Englewood Line. Streets are identified from west to east.

Damen Avenue

Traffic volumes (AADT): 11,900, # of Thru Lanes 2, Lane Width: 11-ft,
Width of Outside Paved Shoulder: 0, Speed Limit: 30 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: < 15%.

Wood Street

Traffic volumes (AADT): Unknown, # of Thru Lanes 2, Lane Width: 15-ft,
Width of Outside Paved Shoulder: 0, Speed Limit: 25 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Poor, % of On-street Parking Occupied: No on-street parking

Ashland Avenue

Traffic volumes (AADT): 18,600, # of Thru Lanes 4, Lane Width: 11-ft, 16-ft,
Width of Outside Paved Shoulder: 0, Speed Limit: 30 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: <10%

Loomis Boulevard

Traffic volumes (AADT): 1450, # of Thru Lanes 2, Lane Width: 16-ft,
Width of Outside Paved Shoulder: 0, Speed Limit: 25 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: N/A

Ada Street

Traffic volumes (AADT): Unknown, # of Thru Lanes 2, Lane Width: 14-ft,
Width of Outside Paved Shoulder: 0, Speed Limit: 25 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: N/A

Throop Street

Traffic volumes (AADT): Unknown, # of Thru Lanes 2, Lane Width: 14-ft

Width of Outside Paved Shoulder: 0, Speed Limit: 25 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: N/A

Elizabeth Street

Traffic volumes (AADT): Unknown, # of Thru Lanes 2, Lane Width: 14-ft,

Width of Outside Paved Shoulder: 0, Speed Limit: 25 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: N/A

Racine Street

Traffic volumes (AADT): 5,700, # of Thru Lanes 2, Lane Width: 16-ft,

Width of Outside Paved Shoulder: 8, Speed Limit: 30 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: <15%

May Street

Traffic volumes (AADT): 225, # of Thru Lanes 2, Lane Width: 16-ft,

Width of Outside Paved Shoulder: 0, Speed Limit: 25 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: <35%

Aberdeen Street

Traffic volumes (AADT): Unknown, # of Thru Lanes 2, Lane Width: 18-ft,

Width of Outside Paved Shoulder: 0, Speed Limit: 25 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: <15%

Morgan Street

Traffic volumes (AADT): Unknown, # of Thru Lanes 2, Lane Width: 18-ft,

Width of Outside Paved Shoulder: 0, Speed Limit: 25 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: <25%

Halsted Street

Traffic volumes (AADT): 10,500, # of Thru Lanes 2, Lane Width: 11-ft,

Width of Outside Paved Shoulder: 0, Speed Limit: 30 mph, % of Heavy Vehicles: unknown,
Pavement Condition : Fair, % of On-street Parking Occupied: <10%

Low Street

Traffic volumes (AADT): Unknown, # of Thru Lanes 2, Lane Width: 16-ft,

Width of Outside Paved Shoulder: 0, Speed Limit: 25 mph, % of Heavy Vehicles: unknown,

Pavement Condition : Fair, % of On-street Parking Occupied: N/A

EXHIBIT 3

Bridge Structure Map

Prepared For:




Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



LEGEND

-  NORFOLK SOUTHERN RAILWAY
-  STRUCTURE
-  NUMBER OF SPANS

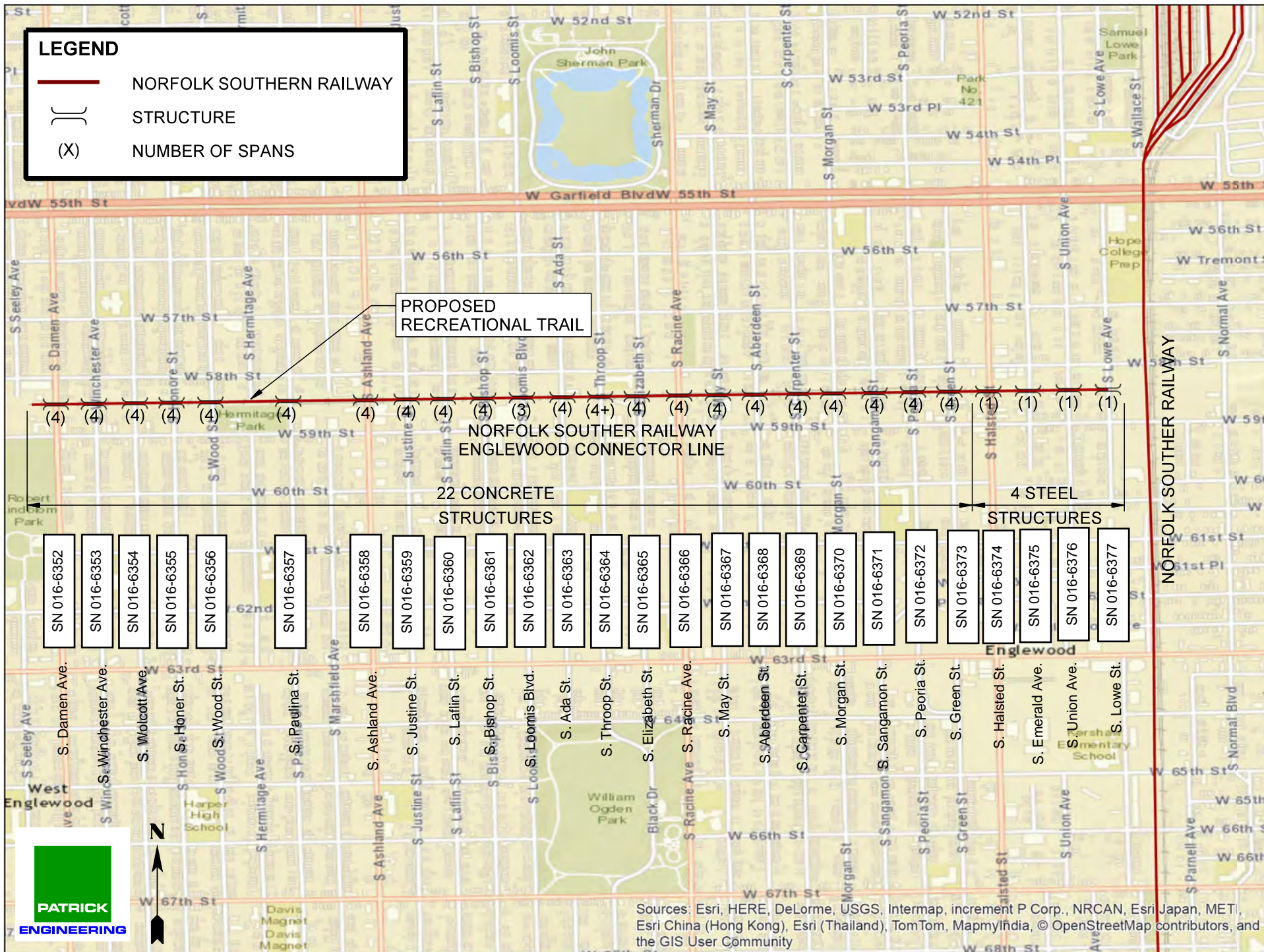


EXHIBIT 4

Structure Master Report & BCR Approval Letter

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



EXHIBIT 5

Design Criteria

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



Design Criteria

Englewood Line



PREPARED FOR:



PREPARED BY:



DATE:

MARCH 2017

Table of Contents

I. Project Summary:..... 2
II. Basis of Design for Shared Used Path:..... 3
III. Pictures of the Current Trail Conditions: 5

I. Project Summary:

LEAD AGENCY: Department of Planning and Development and Department of Transportation
CITY ALDERMANIC WARD: 15, 16, & 20
ROUTE: Norfolk Southern Abandoned Englewood Line
CDOT JOB NUMBER: E-5-482

The City of Chicago proposes to convert the Englewood Connecting Line (Englewood Line) right-of-way into a grade-separated, multi-use trail. The multi-use trail provides connectivity between the West Englewood and Englewood neighborhoods. The Englewood Line is a 1.7 mile long abandoned rail line that was elevated from street grade in 1917 by the Pennsylvania Railroad. The line is oriented east to west between Hoyne Street and Wallace Street south of 58th Street. The project's right-of-way is being transferred by Norfolk Southern Corporation (NS) to the City of Chicago as part of a land exchange agreement.

The proposed trail crosses perpendicularly over twenty-six streets maintained by the City of Chicago. Two of the streets (Ashland and Morgan) are under Cook County jurisdiction, but maintained by the City. The trail is at grade at Hoyne and is built on raised embankment that crosses over Damen Avenue and remains elevated over the 25 streets to the east before returning to grade at Wallace Avenue. Twenty-two of the viaduct structures are concrete simple spans with flat slab reinforced concrete superstructure. The four eastern viaducts over Halsted, Emerald, Union, and Lowe are simple span steel through plate girder superstructures. All structures have been inspected and recommendations for rehabilitation have been proposed.

There are multiple City owned properties adjacent to the trail and several additional parcels owned by NS that are adjacent to the Englewood Line's right-of-way. The adjacent NS properties will be transferred to the City along with the Englewood Line's right-of-way.

The constructed trail fulfills a recommendation for a bicycle trail in this location identified in several plans including the CMAP Green Healthy Neighborhoods Plan, the 2005 Englewood LISC Quality of Life Plan, and the New Era Trail Plan completed in 2009.

II. Basis of Design for Shared Used Path:

General Criteria		
<i>Design Item</i>	<i>Criteria</i>	<i>Source</i>
Path Type	Two-Way, Shared Use	Project Scope
Surface	Paved HMA	CDOT/DPD direction
Pavement Depth	Min. 6"	AASTHO 5.2.9
Design Speed*	12 mph	BLRS 42-3.02(e) Adjusted per Engineering Judgement
Anticipated Volume of Users	>300 per peak hour	Assumed
User Type	Non-recreational bicyclists	Assumed

* Assumes User type to be non-recreational cyclist. Recreational cyclist 85th percentile speed is 18mph

Cross Section Criteria		
<i>Design Item</i>	<i>Criteria</i>	<i>Source</i>
Main trail width*	12 ft.	BLRS 42-3.02(b) Figure 42-3A
Minimum Trail Width at locations of low useage	8 ft. (10 ft minimum on structures)	BLRS Figure 42-3A; AASTHO 5.2.1; BLRS 42-3.02(h)
Maximum Trail Grade	5% (3% if unpaved)	BLRS 42-3.02(g)1; AASTHO 5.2.7
Cross Slope	1%	AASTHO 5.2.6
Shoulder width (unpaved/grass)	Min. 2 ft.	BLRS 42-3.02(b); AASTHO 5.2.1
Shoulder cross slope	4% (6:1 max)	BLRS 42-3.02(b) Figure 42-3B; AASTHO 5.2.1
Distance from edge of paved trail to top of slopes greater than 1V:3H	5 ft (or provide 3.5 ft. physical barrier)	BLRS 42-3.02(d); AASTHO 5.2.1

* Based on the anticipated Volume

Horizontal Criteria		
<i>Criteria</i>	<i>Two-Way, Shared Use</i>	<i>Source</i>
Desirable Minimum Radius (based on 15 deg. Lean angle)	36 ft. (100 ft. for 20 mph.)	BLRS 42-3.02(f) Figure 42-3D
Superelevation (max)	Max. 2%	BLRS 42-3.02(f); AASTHO 5.2.6

Vertical Criteria		
<i>Criteria</i>	<i>Two-Way, Shared Use</i>	<i>Source</i>
Desirable Maximum Grade	5% (Grades in excess of 3.0% need to be evaluated for the need for ADA compliance.)	BLRS 42-3.02(g); AASTHO 5.2.7
Stopping sight distance	$S = V^2 / (30(f \pm G)) + 3.67V$ where V = velocity, mph f = coefficient of friction (use 0.16), G = grade ft/ft	BLRS 42-3.02(g); AASTHO 5.2.8
Bike Path Structures Railings	Railings, fences, or barriers minimum of 54 in, Smooth rub rails 42 in; Opening between horizontal and vertical members of railing small enough so a 6" sphere cannot pass through	BLRS 42-3.02(h); AASTHO 5.2.10

References:

AASTHO Guide for the Development of Bicycle Facilities; 2012; Fourth Edition
Bureau of Local Roads & Streets; Chapter 42 – Bicycle Facilities; October 2013

General Drainage Criteria		
Design Item	Criteria	Source
<u>Bridges</u> Storm drainage system Storm water Detention Sewer Connection	Required. Not required due to location in City ROW. To city sewer main without restrictor.	1. City of Chicago Stormwater Ordinance Manual 2. Project Meeting Minutes with DOB date March 28, 2017
<u>Trail</u> Storm drainage system Storm water Detention Storm Water infiltration Sewer Connection	Required. Required. 0.5 Inch/ hour allowed based on soil borings without infiltration testing. Higher infiltration rate may be allowed based on infiltration testing. If necessary to city sewer main with restrictor.	1. City of Chicago Stormwater Ordinance Manual 2. Project Meeting Minutes with DOB date March 28, 2017 3. Soil Borings
<u>Access point to trail</u> Storm drainage system Storm water Detention Storm Water infiltration Sewer Connection	Required. Required Allowed based on infiltration testing. If necessary to city sewer main with restrictor	1. City of Chicago Stormwater Ordinance Manual 2. Project Meeting Minutes with DOB date March 28, 2017



LIGHTING LEVEL RECOMMENDATION SUMMARY

Project Name CDOT Englewood Line Date: 4/3/2017
 Project Number: 5130 Project Engineer: Oren Skidelsky

Location	IES Literature	Illuminance Horizontal Eh	Illuminance Vertical Ev (5ft AFG)	Illuminance Horizontal Eh	Illuminance Horizontal Eh	Illuminance Vertical Ev (5ft AFG)	Illuminance Horizontal Eh
		Average (fc)		Max (fc)	Min (fc)		Avg/Min
Pedestrian Bike Path	IES Manual 8th Ed.						
Type A (Roadside)							
Commercial Areas		1		--	--	--	--
Intermediate Areas		0.6		--	--	--	--
Residential Areas		0.2		--	--	--	--
Type B (Distant from Roadways)		0.5		--	--	--	--
Pedestrian Bike Path Tunnel		4.3		--	--	--	--
Parks/Plaza (Table 34.2)	IES Manual 10th Ed.						
High Activity, LZ2		0.4	0.2	--	--	--	--
Medium Activity, LZ2		0.2	0.1	--	--	--	--
Low Activity, LZ2		0.1	0	--	--	--	--
Ramps, Stairs and Steps (Table 34.2)	IES Manual 10th Ed.						
High Activity, LZ2		0.6	0.2	--	--	--	--
Medium Activity, LZ2		0.4	0.2	--	--	--	--
Low Activity, LZ2		0.2	0.1	--	--	--	--
Roadway/Underpass	RP-8-14			--	--	--	--
Table 4: High Pedestrian Conflict Area							
Mixed Vehicle and Pedestrian		2	--	--	--	1	4
Pedestrian Only		1	--	--	--	0.5	4
Table 7: Ped portion of Ped/Veh. Underpasses							
Day		10	--	--	--	5	3
Night		4	--	--	--	2	3

III. Pictures of the Current Trail Conditions:



5 Typical view along proposed trail



4 Typical view along proposed trail



6 Typical view along proposed trail



3 Typical view along proposed trail



2 Typical view along proposed trail



1 Typical view along proposed trail



10 Union Street Viaduct



9 Lowe Street



12 Emerald Avenue Viaduct



8 Emerald Avenue Auxiliary Structure



11 Halsted Street Viaduct



7 Halsted Street Viaduct



21 Green Street Viaduct



22 Green Street Viaduct



20 Peoria Street Viaduct



19 Peoria Street Viaduct



17 Sangamon Street Viaduct



18 Sangamon Street Viaduct



Morgan Street Viaduct



27 View above Morgan Street



26 Carpenter Street Viaduct



25 Carpenter Street Viaduct



24 Aberdeen Street Viaduct



23 Aberdeen Street Viaduct



30 Elizabeth Street Viaduct



31 May Street Viaduct



32 May Street Auxiliary Structure

EXHIBIT 6

Cost Estimate

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



DETAILED ESTIMATE OF COSTS

Project Title: Englewood Line					
Item	Description	Unit	Quantity	Unit Price	Total
BRIDGE REHABILITATION					
1	Rehabilitation of Structure, S. Damen Ave.	EACH	1	\$299,000.00	\$299,000.00
2	Rehabilitation of Structure, S. Winchester Ave.	EACH	1	\$243,000.00	\$243,000.00
3	Rehabilitation of Structure, S. Wolcott Ave.	EACH	1	\$254,000.00	\$254,000.00
4	Rehabilitation of Structure, S. Honore St.	EACH	1	\$236,000.00	\$236,000.00
5	Rehabilitation of Structure, S. Wood St.	EACH	1	\$221,000.00	\$221,000.00
6	Rehabilitation of Structure, S. Paulina St.	EACH	1	\$243,000.00	\$243,000.00
7	Rehabilitation of Structure, S. Ashland Ave.	EACH	1	\$486,000.00	\$486,000.00
8	Rehabilitation of Structure, S. Justine St.	EACH	1	\$242,000.00	\$242,000.00
9	Rehabilitation of Structure, S. Laflin St.	EACH	1	\$227,000.00	\$227,000.00
10	Rehabilitation of Structure, S. Bishop St.	EACH	1	\$242,000.00	\$242,000.00
11	Rehabilitation of Structure, S. Loomis Blvd.	EACH	1	\$273,000.00	\$273,000.00
12	Rehabilitation of Structure, S. Ada St.	EACH	1	\$260,000.00	\$260,000.00
13	Rehabilitation of Structure, S. Throop St.	EACH	1	\$246,000.00	\$246,000.00
14	Rehabilitation of Structure, S. Elizabeth St.	EACH	1	\$381,000.00	\$381,000.00
15	Rehabilitation of Structure, S. Racine Ave.	EACH	1	\$369,000.00	\$369,000.00
16	Rehabilitation of Structure, S. May St.	EACH	1	\$317,000.00	\$317,000.00
17	Rehabilitation of Structure, S. Aberdeen St.	EACH	1	\$255,000.00	\$255,000.00
18	Rehabilitation of Structure, S. Carpenter St.	EACH	1	\$203,000.00	\$203,000.00
19	Rehabilitation of Structure, S. Morgan St.	EACH	1	\$255,000.00	\$255,000.00
20	Rehabilitation of Structure, S. Sangamon St.	EACH	1	\$206,000.00	\$206,000.00
21	Rehabilitation of Structure, S. Peoria St.	EACH	1	\$224,000.00	\$224,000.00
22	Rehabilitation of Structure, S. Green St.	EACH	1	\$240,000.00	\$240,000.00
23	Rehabilitation of Structure, S. Halsted St.	EACH	1	\$282,000.00	\$282,000.00
24	Rehabilitation of Structure, S. Emerald Ave.	EACH	1	\$316,000.00	\$316,000.00
25	Rehabilitation of Structure, S. Union Ave.	EACH	1	\$272,000.00	\$272,000.00
26	Rehabilitation of Structure, S. Lowe Ave.	EACH	1	\$246,000.00	\$246,000.00
TOTAL:					\$7,038,000.00
RETAINING WALL REHABILITATION					
27	Wall #1 – Damen Ave.	SQ FT	1000	\$75.00	\$75,000.00
28	Wall #2 – Damen Ave.	SQ FT	600	\$75.00	\$45,000.00
29	Wall #3 – Wood St.	SQ FT	600	\$75.00	\$45,000.00
30	Wall #4 – Hermitage Park	SQ FT	900	\$125.00	\$112,500.00
31	Wall #5 – Hermitage Park	SQ FT	700	\$75.00	\$52,500.00
32	Wall #6 – Hermitage Park	SQ FT	700	\$75.00	\$52,500.00
33	Wall #7 – Ashland Ave.	SQ FT	400	\$75.00	\$30,000.00
34	Wall #8 – Loomis Blvd.	SQ FT	400	\$75.00	\$30,000.00
35	Wall #9 – Racine Ave.	SQ FT	2200	\$125.00	\$275,000.00

ESTIMATES ARE BASED UPON QUANTITIES AND UNIT COSTS WHENEVER POSSIBLE.

DETAILED ESTIMATE OF COSTS

Project Title: Englewood Line					
Item	Description	Unit	Quantity	Unit Price	Total
36	Wall #10 – Racine Ave.	SQ FT	200	\$75.00	\$15,000.00
37	Wall #11 – Morgan St.	SQ FT	1900	\$125.00	\$237,500.00
38	Wall #12 – Morgan St.	SQ FT	200	\$75.00	\$15,000.00
39	Wall #13 – Morgan St.	SQ FT	300	\$75.00	\$22,500.00
40	Wall #14 – S Halsted St.	SQ FT	500	\$75.00	\$37,500.00
41	Wall #15 – S Halsted St.	SQ FT	500	\$75.00	\$37,500.00
42	Wall #16 – N Halsted St.	SQ FT	600	\$75.00	\$45,000.00
43	Wall #17 – E End	SQ FT	200	\$75.00	\$15,000.00
TOTAL:					\$1,142,500.00
SITE CLEARING AND PREPARATION					
44	Tree Removal (6 to 15 Units Diameter)	UNIT	20	\$50.00	\$1,000.00
45	Earth Excavation	CU YD	16420	\$50.00	\$821,000.00
46	Removal and Disposal of Unsuitable Material	CU YD	4000	\$80.00	\$320,000.00
47	Ballast Removal	CU YD	8440	\$25.00	\$211,000.00
48	Building Removal, No. 1	L SUM	1	\$250,000.00	\$250,000.00
49	Concrete Removal (Auxiliary Structures)	CU YD	414	\$3,500.00	\$1,449,000.00
50	Clearing (Special)	ACRE	2.8	\$10,000.00	\$28,000.00
51	Geotechnical Fabric for Ground Stabilization	SQ YD	39160	\$4.00	\$156,640.00
52	Borrow Excavation	CU YD	10000	\$20.00	\$200,000.00
53	Furnished Excavation	CU YD	10000	\$20.00	\$200,000.00
54	Erosion Control Blanket	SQ YD	25000	\$2.00	\$50,000.00
TOTAL:					\$3,686,640.00
LANDSCAPING					
55	Topsoil Furnish and Place, 4”	SQ YD	15120	\$20.00	\$302,400.00
56	Seeding, Class 2A	ACRE	3.2	\$5,000.00	\$16,000.00
57	Nitrogen Fertilizer Nutrient	POUND	282	\$15.00	\$4,230.00
58	Phosphorous Fertilizer Nutrient	POUND	282	\$15.00	\$4,230.00
59	Potassium Fertilizer Nutrient	POUND	282	\$15.00	\$4,230.00
60	Supplemental Watering	UNIT	772	\$72.12	\$55,613.17
TOTAL:					\$386,703.17
PATHWAY					
61	Sidewalk Removal	SQ FT	1650	\$5.00	\$8,250.00
62	Portland Cement Concrete Sidewalk, 5 Inch	SQ FT	1885	\$7.01	\$13,213.85
63	Class C Patches, Type I, 7 Inch	SQ YD	100	\$205.92	\$20,592.00
64	Class C Patches, Type II, 7 Inch	SQ YD	100	\$200.00	\$20,000.00
65	Class C Patches, Type III, 7 Inch	SQ YD	100	\$150.00	\$15,000.00

ESTIMATES ARE BASED UPON QUANTITIES AND UNIT COSTS WHENEVER POSSIBLE.

DETAILED ESTIMATE OF COSTS

Project Title: Englewood Line					
Item	Description	Unit	Qty.	Unit Price	Total
66	Class C Patches, Type IV, 7 Inch	SQ YD	100	\$150.00	\$15,000.00
67	Class C Patches, Type IV, 10 Inch	SQ YD	40	\$214.94	\$8,597.60
68	Bituminous Materials (Prime Coat)	TON	6	\$631.37	\$3,788.22
69	Subbase Granular, Type B 4"	SQ YD	16521	\$15.00	\$247,815.00
70	Aggregate Base Course, Type A	CU YD	5418	\$42.00	\$227,556.00
71	Mixture for Cracks, Joints, and Flangeways	TON	26	\$364.25	\$9,470.50
72	Hot-Mix Asphalt Binder Course, N30 (Low ESAL)	TON	960	\$130.00	\$124,800.00
73	Hot-Mix Asphalt Surface Course, Mix "C", N30 (Low ESAL)	TON	1940	\$125.00	\$242,500.00
74	Thermoplastic Pavement Marking – Line 4"	FOOT	31000	\$2.00	\$62,000.00
TOTAL:					\$1,018,583.17
DRAINAGE					
75	Filter Fabric	SQ YD	54085	\$6.00	\$324,510.00
76	Subbase Granular Material, Type C	CU YD	12320	\$60.00	\$739,200.00
77	Pipe Underdrains, Type 1, 4"	FOOT	3225	\$60.00	\$193,500.00
78	Pipe Underdrain Fabric Lined Trench 6"	FOOT	7150	\$65.00	\$464,750.00
79	4'-Diameter Catch Basin with Frame and Lid	EACH	39	\$4,500.00	\$175,500.00
80	6" Cleanout	EACH	71	\$750.00	\$53,250.00
81	Soil Media Mix	CU YD	11455	\$60.00	\$687,300.00
82	Storm Sewer, Ductile Iron Pipe 4"	FOOT	1250	\$20.00	\$25,000.00
83	Storm Sewer, Ductile Iron Pipe w/ Pavement Restoration	FOOT	540	\$22.00	\$11,880.00
84	Proposed Storm Sewer Connection to Existing Storm Sewer	EACH	12	\$1,500.00	\$18,000.00
85	Trench Drain	FOOT	20	\$150.00	\$3,000.00
TOTAL:					\$2,695,890.00
SIGNAGE					
86	Project Sign	EACH	11	\$366.10	\$4,027.10
87	Sign Panel-Type 1	SQ FT	100	\$25.00	\$2,500.00
88	Sign Panel-Type 2	SQ FT	40	\$30.00	\$1,200.00
89	Sign Post	EACH	200	\$250.00	\$50,000.00
TOTAL:					\$57,727.10
LIGHTING					
90	Lighting Pole Foundation 18"	EACH	432	\$1,500.00	\$648,000.00
91	Multi-Use Trail lighting Pole F1-A	EACH	400	\$7,000.00	\$2,800,000.00
92	Multi-Use Trail Lighting Pole F1-A1	EACH	32	\$7,000.00	\$224,000.00
93	Electrical Cable 1/C #6 AWG in Conduit	FOOT	18000	\$20.00	\$360,000.00
94	Electrical Handhole	EACH	60	\$2,800.00	\$168,000.00
95	Lighting Controller	EACH	6	\$20,000.00	\$120,000.00

ESTIMATES ARE BASED UPON QUANTITIES AND UNIT COSTS WHENEVER POSSIBLE.

DETAILED ESTIMATE OF COSTS

Project Title: Englewood Line					
Item	Description	Unit	Quantity	Unit Price	Total
96	Electrical Service Connection with Metering	EACH	6	\$30,000.00	\$180,000.00
97	PVC Conduit 1" Schedule-40 Direct Burial	FOOT	9000	\$15.00	\$135,000.00
98	PVC Conduit 1" Schedule-80 Direct Burial	FOOT	200	\$30.00	\$6,000.00
99	Ground Rod	EACH	18	\$2,500.00	\$45,000.00
100	#2 Ground Wire	FOOT	150	\$45.00	\$6,750.00
101	Electrical Cable 1/C #10 AWG in Conduit	FOOT	200	\$3.00	\$600.00
102	Conduit 1" GRC	FOOT	60	\$13.00	\$780.00
103	Electrical NEMA 4X Junction Box	EACH	6	\$1,200.00	\$7,200.00
TOTAL:					\$4,701,330.00
ENVIRONMENTAL REMEDIATION					
104	Geotextile (Murafi-180N)	SQ YD	26000	\$5.00	\$130,000.00
105	Stone	CU YD	13000	\$22.00	\$286,000.00
106	In situ Maectile Treatment	CU YD	50	\$200.00	\$10,000.00
TOTAL:					\$426,000.00
EXTRA					
107	Handrail – Path Ramp	FOOT	794	\$175.00	\$138,950.00
108	Fire Hydrant Assembly Complete	EACH	3	\$8,875.29	\$26,625.87
109	Drinking Fountain Assembly	EACH	11	\$8,880.00	\$97,680.00
TOTAL:					\$263,255.87
CONSTRUCTION OVERHEAD					
110	Traffic Control and Protection (Detour)	L SUM	1	\$850,000.00	\$850,000.00
111	Traffic Control and Protection, (Special)	L SUM	1	\$700,000.00	\$700,000.00
112	Engineer's Field Office, Type A	CAL MO	12	\$5,000.00	\$60,000.00
113	Mobilization	L SUM	1	\$3,500,000.00	\$3,500,000.00
114	Construction Layout	L SUM	1	\$830,000.00	\$830,000.00
TOTAL:					\$5,940,000.00
ACTIVITY SITES					
115	Access Point, Activity Site – Hoyne Avenue	EACH	1	\$1,000,000.00	\$1,000,000.00
116	Access Point, Activity Site – Ashland Avenue	EACH	1	\$500,000.00	\$500,000.00
117	Access Point, Activity Site – Ada Street	EACH	1	\$500,000.00	\$500,000.00
118	Access Point, Activity Site – Halsted Street	EACH	1	\$1,000,000.00	\$1,000,000.00
119	Access Point, Activity Site – Lowe Street	EACH	1	\$250,000.00	\$250,000.00
TOTAL:					\$3,250,000.00

ESTIMATES ARE BASED UPON QUANTITIES AND UNIT COSTS WHENEVER POSSIBLE.

DETAILED ESTIMATE OF COSTS

Project Title: Englewood Line					
Item	Description	Unit	Quantity	Unit Price	Total
SUBTOTAL:					\$30,606,629
Contingency 20%:					\$6,121,326
SUBTOTAL:					\$36,727,955
Construction and Design Engineering 20%:					\$7,345,591
RIGHT-OF-WAY ACQUISITION					
1	ROW Acquisition, Hoyne Avenue 20-18-124-001	SQ FT	16126	\$4.23	\$68,213
2	ROW Acquisition, Ashland Avenue 20-17-124-008	SQ FT	3050	\$4.15	\$12,672
3	ROW Acquisition, Ashland Avenue 20-17-124-009	SQ FT	3050	\$4.23	\$12,888
4	ROW Acquisition, Ashland Avenue 20-17-124-010	SQ FT	6100	\$4.06	\$24,744
5	ROW Acquisition, Ashland Avenue 20-17-124-027	SQ FT	3300	\$2.65	\$8,742
6	ROW Acquisition, Ashland Avenue 20-17-124-028	SQ FT	6600	\$2.25	\$14,866
7	ROW Acquisition, Ashland Avenue 20-17-124-029	SQ FT	3300	\$3.26	\$10,750
8	ROW Acquisition, Ashland Avenue 20-17-124-034	SQ FT	3721	\$4.22	\$15,718
9	ROW Acquisition, Ashland Avenue 20-17-124-035	SQ FT	1220	\$1.50	\$1,830
10	ROW Acquisition, Ashland Avenue 20-17-124-037	SQ FT	5346	\$1.91	\$10,224
11	ROW Acquisition, Damen Avenue 20-18-224-017	SQ FT	2976	\$0.70	\$2,082
12	ROW Acquisition, Damen Avenue 20-18-224-018	SQ FT	3100	\$0.70	\$2,170
13	ROW Acquisition, Damen Avenue 20-18-224-019	SQ FT	3100	\$0.70	\$2,170
14	ROW Acquisition, Damen Avenue 20-18-224-020	SQ FT	3100	\$0.70	\$2,170
TOTAL:					\$189,239
TOTAL COST OF ITEMS:					\$44,262,785

ESTIMATES ARE BASED UPON QUANTITIES AND UNIT COSTS WHENEVER POSSIBLE.

EXHIBIT 7

Table of Required Right-Of-Way & Easements

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



TABLE OF REQUIRED RIGHT-OF-WAY

Project Title: Englewood Line					
PIN	SQ FT	Assessed Value	Cost per SQ FT	Acres	Street
20-18-224-017	2,976	\$1,041	\$0.35	0.0683	DAMEN
20-18-224-018	3,100	\$1,085	\$0.35	0.0712	DAMEN
20-18-224-019	3,100	\$1,085	\$0.35	0.0712	DAMEN
20-18-224-020	3,100	\$1,085	\$0.35	0.0712	DAMEN
20-17-124-008	3,050	\$6,336	\$2.08	0.0700	ASHLAND
20-17-124-009	3,050	\$6,444	\$2.11	0.0700	ASHLAND
20-17-124-010	6,100	\$12,372	\$2.03	0.1400	ASHLAND
20-17-124-027	3,300	\$4,371	\$1.32	0.0758	ASHLAND
20-17-124-028	6,600	\$7,433	\$1.13	0.1515	ASHLAND
20-17-124-029	3,300	\$5,375	\$1.63	0.0758	ASHLAND
20-17-124-034	3,721	\$7,859	\$2.11	0.0854	ASHLAND
20-17-124-035	1,220	\$915	\$0.75	0.0280	ASHLAND
20-17-124-037	5,346	\$5,112	\$0.96	0.1227	ASHLAND
20-18-124-001	-	-	-	0.3402	HOYNE
TOTAL:		\$60,513		1.4413	

SQUARE FOOTAGE AND ASSESSED VALUE OBTAINED FROM COOK COUNTY ASSESSOR'S OFFICE.



**COOK COUNTY
ASSESSOR'S OFFICE**

Joseph Berrios

EXHIBIT 8

ESA Memorandum Summary

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



SUMMARY OF PHASE I & II ENVIRONMENTAL ASSESSMENT REPORTS

Englewood Connector, Chicago Illinois

The City of Chicago (the City), Department of Fleet and Facility Management contracted Tetra Tech Inc. (Tetra Tech) to perform Phase I and Phase II Environmental Site Assessments of approximately 1.92 miles of an elevated railroad embankment in the Englewood neighborhood of Chicago, Cook County. The studies were performed in 2015, and were designed to identify and characterize the environmental conditions at the Site, and estimate a range of environmental costs associated with addressing these conditions during a proposed construction project in the area.

Thirty (30) boring locations were associated with the Phase II, and nearly 70 soil samples were submitted for laboratory analysis. Approximately 20 constituents of concern (COC) were identified at the subject property exceeding Illinois screening standards, primarily polynuclear aromatic hydrocarbons (PAHs) and metals, very typical chemical constituents found in historically industrial areas.

Patrick's review of the resulting data indicates that the contaminants appear to be randomly distributed across the various soil-boring locations; most boring locations contained at least one COC that exceeded the screening standard for at least one exposure pathway. Such a random distribution is commonly found in historically industrial areas; there do not appear to be "hot spots" associated with any particular historical activity or spill event. As such, a focused remedial approach, based upon a limited excavation of specific areas, does not appear practical here. On the other hand, removal of all of the impacted soils (as estimated on the basis of somewhat limited data) would be excessive and impractical (over 40,000 tons of soil, costing in excess of \$2.5 million).

In the Phase II report, Tetra Tech suggests three additional alternatives to the complete removal approach described above. The second of these alternatives would remove only the top three feet of impacted soils, and replace them with clean fill for the entire width of the 30-

foot-wide elevated railway. Lead-impacted soils which are considered hazardous (based upon the results of a leachability test) would be chemically stabilized regardless of depth, and left in place. This technical approach is explicitly allowed under the Illinois EPA's Site Remediation Program (SRP), and would likely be successful and acceptable to the Illinois regulators. This approach was estimated by Tetra Tech to cost \$582k.

A third approach suggested by Tetra Tech would be similar the preceding alternative, but instead of excavating impacted soils to a depth of 3 feet, only 18 inches would be removed and replaced by a geotextile fabric overlain by 18 inches of clean stone (across the entire 30-foot width of the elevated railway). This approach is not explicitly allowed within the regulations governing the Illinois SRP, but such an approach has been approved by Illinois EPA in the past (Patrick has itself had had such projects approved in the recent past). As there is a strong precedent for Illinois EPA approval, this approach is reasonable, and is somewhat less expensive than the second alternative; Tetra Tech estimates a total cost of \$426k.

Tetra Tech also offered a fourth alternative. This last alternative is the same as the preceding approach, except that instead of removing soils across the entire 30-foot width of the railway, only an 8-foot zone would be cleared, along the footpath alignment. This footpath would include some degree of restriction for off-path use (signage, etc.), and would result in a much lower excavation cost. However, Patrick does not anticipate that the Illinois EPA would agree to such an approach. In this case, an engineered barrier (the stone) would be used in conjunction with an institutional control (off-path restrictions) in a way that does not have any known precedent. Illinois EPA is likely to raise the issue that park goers may very well ignore posted signs, and come into contact with contaminated soils, making the institutional control ineffective and unenforceable. For this reason, Patrick regards this alternative as carrying an unacceptable degree of regulatory risk. Patrick recommends the third excavation alternative be implemented. A conservative remediation cost of \$500k may be assumed, with the caveat that it is based on the relatively limited amount of data collected from the Site (only 30 borings installed along a nearly two-mile length of railway).

EXHIBIT 9

Clearances of Environmental Resources

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:





Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

LOCAL ROADS AND STREETS

Cultural Clearance Notification

City of Chicago

Location: Englewood Elevated Railroad Connector (59th St.),
Hoyne Avenue to Wallace Avenue

Section No.: 16-E5482-00-BT

CDOT Project No.: E-5-482

Cook County

April 24, 2017

Mr. Daniel F. Burke, P.E., S.E.
Deputy Commissioner
City of Chicago
Department of Transportation
30 North LaSalle Street, Suite 400
Chicago, IL 60602-2570

Attn: Mr. Luis D. Benitez, P.E., S.E.

Dear Mr. Burke:

Enclosed is the Cultural Clearance for the above-referenced project, which should be included in the Project Development Report.

If you have any questions or need additional information, please contact Zubair Haider, Field Engineer, at (847) 705-4206 or via email at Zubair.Haider@illinois.gov.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer

By: 
Christopher J. Holt, P.E.
Bureau Chief of Local Roads and Streets

Enclosure



Illinois Department of Transportation

Memorandum

To: Maureen Kastl Attn: William Raffensperger
From: Maureen Addis By: Brad Koldehoff
Subject: Cultural Resource Clearance
Date: April 20, 2017

Cook County
Englewood Trail, Chicago
Sec. 16-E5482-00-BT
Seq. 20477

For the above referenced project, IDOT's qualified professional Cultural Resources staff hereby make a **"No Historic Properties Affected"** finding pursuant to Section 106 of the National Historic Preservation Act.

This determination follows the stipulations of the Section 106 Programmatic Agreement for the Delegation of Authority for Minor Projects of the Federal Aid Highway Program in the State of Illinois, executed by FHWA, Illinois SHPO, IDOT and the Advisory Council on Historic Preservation. This project is consistent with the minor project types listed in Appendix B of the agreement.

The attached stamped IDOT Environmental Survey Request form documents that no further coordination for this project is required for cultural resources.

Attachment

BK:km

Environmental Survey Request

A. Project Information

Bio Cultural Wetlands Special Waste

Submittal Date: 02/02/2017 Sequence No: 20477
District: 1 Requesting Agency: Local CDOT Project No:
Contract #: Job No.:
Counties: Cook
Route: Englewood Trail Marked:
Street: Section: 16-E5482-00-BT
Municipality(ies) Chicago Project Length: 3.219 km 2 miles
From To (At): Hoyne Avenue (West) to east of Lowe Avenue
Quadrangle: Englewood Township-Range-Section: T 38N R 14E
Survey Completion Target Date 08/23/2017 Anticipated Design Approval: 11/01/2017 Anticipated NEPA Processing: CE

B. Reason for Submittal: (Check all that apply); Includes SW Level 1 Screening Criteria

Survey Types: B = Biological; C = Cultural; SW = Special Waste

B,C,SW Involves Acquisition of additional ROW or temporary or permanent easements. 4.451535 ha/ 11 acres
SW On a state-maintained route, crosses or involves RR ROW (except a single rail rural ROW with no maintenance facilities).
B,C Requires In-Stream work (e.g., drainage structure runaround). Stream Name:
C Potential to affect a historic district or historic property.
C Involves replacement or rehabilitation of a bridge/culvert 40 years old or older.
SW Involves acquisition of, excavation (defined in BDE Manual 27-3.01) on, or subsurface utility relocation on State ROW.
 Other: Conversion of abandoned RR (Englewood Line) ROW to a linear park/recreational trail

C. Project Description:

CDOT is performing a Phase I Study for the potential repurposing of an abandoned RR ROW into a linear recreational trail. The RR ROW is located between 58th and 59th Streets in Chicago

Proposed Work: Roadway Bridge Railroad Airport Other Shared-Use Path

Tree Removal?: Yes Number?: 20 ha/ acres

Existing Bridge(s) Structure Number:	016-6353	On Historic Bridge List:	No
Existing Bridge(s) Structure Number:	016-6374	On Historic Bridge List:	No
Existing Bridge(s) Structure Number:	016-6377	On Historic Bridge List:	No

Historic District Involved? No Historic Buildings Involved? No

Section 4(f) Lands Involved? Yes Section 6(f) Lands Involved? No

Wetland delineation performed by: End. Species Consultation performed by: BDE

D. Funding: Federal State TBP MFT Local Non-MFT Other
 404 Permit Required

E. District Contact: Zubair Haider Local Contact: Moira Kent
Telephone #: (847) 705-4206 ext. Telephone #: (312) 744-9458 ext.
Env. Contact: E-Mail: moira.kent@cityofchicago.org
Telephone #: Title/Company:

Closed PSI/RMP Only ESR Rec'd in CO SW Rec'd

CULTURAL RESOURCES:
NO SURVEY OR FURTHER COORDINATION REQUIRED
Bob Kallehoff 4/20/17
SIGNED DATE

Project Overview

Submittal Date: 02/02/2017 **Sequence No:** 20477
District: 1 **Requesting Agency:** Local **CDOT** **Project No.:** _____
Contract #: _____ **Job No.:** _____
Counties: Cook
Route: Englewood Trail **Marked:** _____
Street: _____ **Section:** 16-E5482-00-BT
Municipality(ies): Chicago **Project Length:** 3.2187 km **2 miles**
FromTo (At): Hoyne Avenue (West) to east of Lowe Avenue
Quadrangle: Englewood **Township-Range-Section:** T 38N R 14E
Survey Target Date: 08/23/2017 **Anticipated Design Appr.:** 11/01/2017 **Anticipated Processing:** CE
Funding: Federal State TBP MFT Local Non-MFT

Consultant: _____
PTB No.: _____ **Item No.:** _____ **PTB Date:** _____ **Prequal Level:** _____

Sequence No:	20477	Biological	Wetlands	Cultural	Special Waste
Entered By		BDE		BDE	
Cleared for DA				4/20/2017	
Cleared for Letting				4/20/2017	
Resubmittal					
ResubmittalCleared					
Section:	16-E5482-00-BT				
Job No.:					
FromTo (At):	Hoyne Avenue (West) to east of Lowe Avenue				

Notice of Intent	Project Initiation Ltr to FHWA	Public Info Meeting(s)		Notice of Availability		Public Hearing	Draft	ROD/FONSI Approved
		1st	2nd	Draft	Final			

Project Phase Comments: _____



Illinois Department of Transportation

Office of Highways Project Implementation / Region 1 / District 1
201 West Center Court / Schaumburg, Illinois 60196-1096

LOCAL ROADS AND STREETS

Biological Clearance Notification

City of Chicago

Location: Englewood Elevated Railroad Connector (59th St.),
Hoyne Avenue to Wallace Avenue

Section No.: 16-E5482-00-BT

CDOT Project No.: E-5-482

Cook County

July 21, 2017

Mr. Daniel F. Burke, P.E., S.E.
Deputy Commissioner
City of Chicago
Department of Transportation
30 North LaSalle Street, Suite 400
Chicago, IL 60602-2570

Attn: Mr. Luis D. Benitez, P.E., S.E.


Dear Mr. Burke:

Enclosed is the Biological Clearance for the above-referenced project, which should be included in the Project Development Report.

If you have any questions or need additional information, please contact Zubair Haider, Field Engineer, at (847) 705-4206 or via email at Zubair.Haider@illinois.gov.

Very truly yours,

Anthony J. Quigley, P.E.
Region One Engineer

By: 
Christopher J. Holt, P.E.
Bureau Chief of Local Roads and Streets

Enclosure

Project Overview

Submittal Date: 02/02/2017 **Sequence No:** 20477
District: 1 **Requesting Agency:** Local **CDOT** **Project No:** _____
Contract #: _____ **Job No.:** _____
Counties: Cook
Route: Englewood Trail **Marked:** _____
Street: _____ **Section:** 16-E5482-00-BT
Municipality(ies): Chicago **Project Length:** 3.2187 km 2 miles
From To (At): Hoyne Avenue (West) to east of Lowe Avenue
Quadrangle: Englewood **Township-Range-Section:** T 38N R 14E
Survey Target Date: 08/23/2017 **Anticipated Design Appr.:** 11/01/2017 **Anticipated Processing:** CE
Funding: Federal State TBP MFT Local Non-MFT

Consultant: _____
PTB No.: _____ **Item No.:** _____ **PTB Date:** _____ **Prequal Level:** _____

Sequence No:	20477	Biological	Wetlands	Cultural	Special Waste
Entered By		BDE		BDE	
Cleared for DA		7/20/2017		4/20/2017	
Cleared for Letting				4/20/2017	
Resubmittal					
ResubmittalCleared					
Section:	16-E5482-00-BT				
Job No.:					
From To (At):	Hoyne Avenue (West) to east of Lowe Avenue				

Notice of Intent	Project Initiation Ltr to FHWA	Public Info Meeting(s)		Notice of Availability		Public Hearing	Draft	ROD/FONSI Approved
		1st	2nd	Draft	Final			

Project Phase
Comments: _____

Biological Resources

Submittal Date: 02/02/2017 **Sequence No:** 20477
District: 1 **Requesting Agency:** Local CDOT
Counties: Cook
Route: Englewood Trail **Marked:** _____
Street: _____ **Section:** 16-E5482-00-BT
Municipality(ies): Chicago **Project Length:** 2 miles
From To (At): Hoyne Avenue (West) to east of Lowe Avenue
Township-Range-Section: T 38N R 14E **Cleared for Design Apprvl:** 07/20/2017
Cleared for Letting: _____

Acquisition of additional ROW or easement 11 acres
 In-Stream Work **Stream Name:** _____
 Other: Conversion of abandoned RR (Englewood Line) ROW to a linear park/recreational trail

Tree Removal?: Yes Number?: 20 acres

Biological Sign Off: _____ **Field Sign Off:** _____ **District Sign Off:** _____
Wetland Sign Off: _____ **Surveys Performed:** _____ **Commitments:** _____

BRR										
District Notified	IDNR Notified	USFWS Notified	NPS Notified	IDNR Response		USFWS Response	NPS Response	District Notified		
				Comments	Concurrence			IDNR	USFW	NPS
07/20/2017	<input type="checkbox"/>	<input checked="" type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>			<input type="checkbox"/>	<input type="checkbox"/>	<input type="checkbox"/>

ITA Type: _____ **Translocation** _____

Comments: mature trees along former RR corridor; 4(d) (SDH)

Endangered Species Consultation					
NRRT (Natural Resources Review Tool) 07/20/2017					
EcoCAT (Ecological Compliance Assessment Tool)					
Submitted	Initial Consultation		Final Consultation		NRRT(OLD)
	Terminated		Terminated		
Resubmitted	Consultation Renewal		NRRT or EcoCAT:		
	Terminated				

Biological Assessment							
IDNR Notified	USFWS Notified	IDNR Response	USFWS Response	District Notified		IDNR Consultation	USFWS Opinion
				IDNR	USFWS		
				<input type="checkbox"/>	<input type="checkbox"/>		

Comments: TREC shows no resources other than DU wetlands in small portion of western part of project (KCK); 7/20/17: updated 3/9/17 TREC, no changes (SDH)

Further Studies	Federal Species *	Tasked	Report Due Date	Results Received
Bio/Cover Type:				
Mammals:	<input type="checkbox"/>			
Birds:	<input type="checkbox"/>			
Plants:	<input type="checkbox"/>			
Herps:	<input type="checkbox"/>			
Fish:	<input type="checkbox"/>			
Mussels:	<input type="checkbox"/>			
Inverts:	<input type="checkbox"/>			
Other:				

Comments: _____

EXHIBIT 10

Categorical Exclusion

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:





Level Two Design Criteria Checklist

Key Route: Englewood Elevated Railroad Connector

Marked Route/Road Name: 59th Street

State Job No.: N/A Contract No.: CD # E-5-487

Functional Classification: N/A Highway Type: N/A

County(ies): Cook Project Length: 1.75 miles

City: Chicago Section: 16-E5482-00BT

Project Location: Between 58th and 59th Streets (N/S), from Hoyne Ave to Wallace Ave

Project Scope of Work

a. Check the appropriate box. See Section 31-6 for definitions.

- | | | | |
|---|--|--|---|
| <input type="checkbox"/> New construction | <input type="checkbox"/> *Reconstruction | <input type="checkbox"/> *3R (non-freeway) | <input type="checkbox"/> *3R (freeway) |
| <input type="checkbox"/> 3P | <input type="checkbox"/> SMART | <input type="checkbox"/> HSIP | <input checked="" type="checkbox"/> Other |

**Note: May include "Allowed to Remain in Place" criteria.*

This form is required for all new construction, reconstruction, and 3R projects.

b. Provide a brief project description:

The proposed Englewood Line multi-use trail is a planned conversion of an abandoned rail line into a multi-use trail. The existing abandoned rail line was elevated from street grade in 1917 by the Pennsylvania Railroad. The proposed trail is oriented in the east-west direction, parallel to and in between 58th and 59th Streets, from Hoyne Avenue on the west to Wallace Avenue on the east.

Design Criteria (Provide numerical values, where indicated.)	Does the proposed design meet the criteria?		
	Yes	No	N/A
1. Basic Design Controls (Chapter 31)			
a. Design speed 12 mph (km/h)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Stopping Sight Distance (SSD) application for vertical curves (downgrade adjusted SSD used)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Truck SSD (level) (at specific sites)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Level of service (mainline)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
2. Horizontal Alignment (mainline) (Chapter 32)			
a. Horizontal curvature (minimum radius for selected design speed) feet (meters)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Superelevation rates ($e_{max} =$ %)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Superelevation transition lengths	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. SSD application at horizontal curves (downgrade adjusted SSD used)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Superelevation distribution between tangent and curve (ratio or percent)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. "Breakover" of outside shoulder on super-elevated curves (percent)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Relative longitudinal slope of shoulder to edge of traveled way on high side of S.E. curve adjacent to bridge with S.E.	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h. Superelevation development at reverse curves	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Design Criteria (Provide numerical values, where indicated.)	Does the proposed design meet the criteria?		
	Yes	No	N/A
i. Is superelevation transition length located off of bridges and bridge approach pavements?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j. Horizontal stopping sight distance on inside of horizontal curves (Level SSD for passenger cars)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
3. Vertical Alignment (mainline) (Chapter 33)			
a. Maximum grades (in percent)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. SSD at crest vertical curves (level SSD for passenger cars)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. SSD at sag vertical curves (level SSD for passenger cars)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Minimum grades (in percent) considering drainage	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Critical length of grade	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Truck-climbing lanes/critical grade analysis	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Design criteria for truck-climbing lanes (e.g., lane width and shoulder width)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h. Minimum length of vertical curves for selected design speed	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Maximum length of vertical curves (drainage of curbed facilities and bridges)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
4. Cross Section Elements (mainline) (Chapter 34)			
a. Lane widths feet (meters)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Design Criteria (Provide numerical values, where indicated.)	Does the proposed design meet the criteria?		
	Yes	No	N/A
m. Outside roadway ditch:			
• Slopes _____ : _____ • Depth _____	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Widths _____	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Median ditch:			
• Slopes _____ • Depth _____ :	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Width _____	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
n. Cross-section transitions into bridges/ underpasses	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o. Use of mountable curbs (V > 45 mph (70 km/h))	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
p. Cross-section transition details (e.g., four-lane to two-lane)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
5. Intersections (Chapter 36)			
a. Accommodation of design vehicle (identify vehicle) _____	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
b. Level of service:			
• Through lanes _____	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Turn lanes _____	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Skew angle	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Profiles	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Volume guidelines for turn-lanes:			
• Right-turns	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Left turns	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Design of right-turn lanes	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
Design of left-turn lanes	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Design Criteria (Provide numerical values, where indicated.)		Does the proposed design meet the criteria?		
		Yes	No	N/A
g. Turn-lane tapers	Approach taper	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	Departure taper	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	Bay taper	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h. Turning roadway widths		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Turn-lane lengths	Deceleration (rural)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	Storage (urban)	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j. Intersection sight distance: List criteria and type _____ _____		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
k. Median opening length _____ feet (meters)		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
l. Minimum corner island size _____ sq. ft (sq. m)		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
m. Does right-turn radius accommodate design vehicle without encroachment?		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
n. Driveway widths _____ feet (meters)		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
o. Type of traffic control:				
• Two-way stop		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• All-way stop		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Traffic signals		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
p. Is maximum grade exceeded on any approach?		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
q. Max. superelevation "e" (in percent) for intersections on curve		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
6. Interchanges (Chapter 37)				
a. Exit terminal	Standard type	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	Design speed of first curve	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	Are any exit terminals located on mainline horizontal curve?	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Design Criteria (Provide numerical values, where indicated.)		Does the proposed design meet the criteria?		
		Yes	No	N/A
b. Entrance terminal	Standard type	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	Length of tangent after the entering curve	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	Design speed of entering curve	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
c. Design speed of ramp proper _____ mph (km/h)		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
d. Design speed of crossroad _____ mph (km/h)		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
e. Maximum ramp grades:		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Exit ramp _____ %		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Entrance ramp _____ %		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
f. Ramp pavement width _____ feet (meters)		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
g. Ramp shoulder widths:		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Left _____ feet (meters)		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Right _____ feet (meters)		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
h. Horizontal ramp curvature in conjunction with selected design speeds		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
i. Superelevation development on ramps	Superelevation rate	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	Transition length	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
	Distribution between tangent & curve	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
j. Vertical curvature compliance with selected design speed on ramp		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
k. Length of access control at crossroad		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
l. Type of traffic control at crossroad:		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Stop signs		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Traffic signals		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
• Free flow		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
m. Is length of crest vertical curve used on crossroad \geq that required by the selected design speed of crossroad?		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>

Design Criteria (Provide numerical values, where indicated.)		Does the proposed design meet the criteria?			
		Yes	No	N/A	
n. Are crossroad approach grades through ramp/crossroad intersections $\leq 2\%$?		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
o. Are ramp/crossroad intersections located on a tangent section of crossroad alignment?		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
p. Is decision sight distance available in advance of exit gore?		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
q. Is clear recovery area available beyond gore nose?		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
r. Level of service:					
• Exit terminal _____		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
• Entrance terminal _____		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
• Ramp proper _____		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
• Weaving area _____		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
• Ramp/crossroad intersection		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
s. Freeway lane drops	Location	Upgrade	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
		Downgrade	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
		Inside lane	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
		Outside lane	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
		At exit terminal	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
		Beyond exit terminal	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
		Taper length	<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>
7. Roadside Safety (Chapter 38)					
a. Horizontal clearances:					
• Clear zones on tangent sections		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
• Clear zones on outside of horizontal curves		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
b. Barrier warrants		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	
c. Barrier length of need		<input type="checkbox"/>	<input type="checkbox"/>	<input checked="" type="checkbox"/>	

EXHIBIT 11

Lighting Basis of Design

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



ENGLEWOOD MULTI-USE TRAIL LIGHTING BASIS OF DESIGN

DESCRIPTION. This work shall consist of furnishing and installing an in-ground pole mounted downlight fixture, Model # RX132-G2-2NA3-15, as manufactured by Philips Industries, and specified on the plans or as directed by the Engineer. The fixture will be complete with a cobra head style area fixture made of die-cast aluminum housing with clear glass lens LED, lamp. The Scope is lighting that will be controlled by six lighting controllers and fed from ComEd from 240 V service with individual metering. Steel lighting poles will be used with a curved shape to give more space at the bike path level.

DESIGN BASIS:

In accordance with the Illuminating Engineering Society of North America (IESNA) Lighting Handbook Tenth Edition (IES-RP-33-14, Table-3), a mixed-use area (pedestrian ways and bike ways) recommended illumination target is a minimum 25 lux (2.5 fc), maximum 65 lux (6.5 fc). This represents the current recommended practice for outdoor lighting and includes criteria for typical applications found in the City of Chicago. The current City of Chicago Department of Transportation (CDOT) Engineering Design Requirements and Guidelines dated Sept. 18, 2015, requires a minimum illumination level equivalent to the IESNA illumination recommendation for this area type plus 50%. The minimum illumination level of 3.75 fc meets CDOT's design criteria. (Minimum 25 lux + .50 (25) = 37.5 lux or 3.75 fc). Given the photometrics and the spacing of the luminaires to reach this minimum illumination level in most areas of the project there is a small percentage of areas where the maximum illumination level is exceeded. There are also a few areas where the minimum illumination falls short of 3.75 fc. This can be expected with a lighting design of this type and it is our opinion that overall this design has met the intent of the City.

1. MATERIALS. Materials shall meet the requirements of the following specifications:

- Housing and Concrete Pour Kit- single piece compression molded fiberglass reinforced polyester composite
- Lens-convex clear high-temperature glass furnished with a molded silicone U-channel gasket for a watertight seal
- Lens ring- single piece compression molded fiberglass reinforced polyester composite
- Rock Guard- single piece compression molded fiberglass reinforced polyester composite direction that controls glare and protects the lens
- Wiring- isolated compartment with cover. Unit furnished with potting compound and moisture absorbent Desiccant pack for moisture free installation
- Socket- Porcelain pulse rate medium base.
- Finish Color- impregnable bronze composite
- Warranty- per manufacturer
- Certification- fixture must be UL listed to US and Canadian safety standards for wet locations. Fixture manufacturer will employ a quality program that is audited to ISO 9001 Standards.

LIGHT FIXTURE

PHILIPS LIGHTING model # RX132-G2-2NA3-15 or approved equal
Driver PHILIPS LUMEC #LRQL0214PMD
RX1, 32 LEDs, 350 ma driver, type 2 optics, clear glass lens
(2) ledgine 3.1 light arrays of 16 LEDs driven at 350 ma
maximum candela = 2633.27275075912 at 72.5 h 67.5 v

IESNA:LM-63-2002

3810 absolute lumens delivered data shown is absolute photometry at rated input
BUG rating b1-u0-g1
Tested in compliance with LM-79-08 procedures

POLE

Provide tapered round galvanized steel pole. Pole shall be fabricated from hot rolled commercial steel (7 Ga) conforming to ASTM A595 grade A. It shall have a continuous taper of 0.14 inches per foot and meet a minimum yield strength of 55,000 PSI. Pole shall be nominal 6" diameter at the bottom and taper to 3" diameter at the top. Pole shall lean back 13 degrees and be bent to achieve a nominal 7'-8 1/4" arm extended into plan. Pole shall be nominal 14' tall and 5'-9 3/4" from the center line of the pole base to the end of the arm. Pole shall be fitted with 2" diameter x 6" long tenon at the end of the arm to accept the luminaire. Provide with flush mounted handhole and cover with tamper-resistant screws. Provide a 1" thick 11"x11" baseplate conforming to ASTM A36 with 4" wiring hole at center and 1/4"x1" backing ring at 13-degree lean back to accept the pole shaft. Pole shaft shall be welded at a 13-degree angle to the baseplate. Baseplate cover shall be two-piece 11 Ga. steel, fastened together with tamper-resistant countersunk 10-24 flathead screws. Provide galvanized leveling nuts at anchor bolts.

NEW ELECTRICAL SERVICE

Provide six new lighting controllers connected to Com Ed service. Locations shown on the drawings are approximate. Each lighting controller shall be individually metered. The electrical service to the lighting controllers shall be connected to the Com Ed overhead line distribution and routed underground to the lighting controllers. Coordinate with Com Ed for proper connection locations. The incoming feeder cables and conduit shall be properly sized for the lighting load plus 35% future capacity. Phase II services shall include all cable and conduit sizes, routing, and handhole locations. All cables shall be sized to include voltage drop to a maximum of 3% from the lighting controller. Load calculations shall be provided for the City of Chicago and Com Ed.

INSTALLATION. Installation shall meet all applicable requirements of Section 1600 and all subsections of the Standard Specifications and all City of Chicago Electrical Code and CDOT requirements. The downlight wire will be spliced to the field wire using splicing methods approved by the Engineer. The downlight will be properly mounted in-ground according to the detailed installation sheet enclosed with each fixture. The contractor shall level and adjust the fixture for proper illumination. The sizing of electrical power feeders to the controllers shall be coordinated with Com Ed.

APPENDIX A

Utility Coordination

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



Refresh



City of Chicago
Department of Transportation
Office of Underground Coordination
30 N. LaSalle St., Suite 310, Chicago, IL 60602
Phone# (312) 744-4828 Fax# (312) 742-3138



Transmittal & Review Form

Status: Initial Review Completed OUC File #: 2017-74659
Client Query #: 70978I Process Date: 02/03/2017
PIN Number: sBVFFLWK Response Required Date: 03/09/2017

Author:

Name: Julius Cousin III
Company: Infrastructure Engineering, Inc.
Address 1: 33 West Monroe Street, Suite 1540
Address 2:
City: Chicago
State: IL
Zip: 60603
Phone: 312-425-9560
Phone Extension: 1274
Fax: 312-425-9564
Mobile:
Email: jcousin@infrastructure-eng.com

Submitting Agency:

Name: Julius Cousin III
Submitting Agency: Infrastructure Engineering, Inc.
Address 1: 33 West Monroe Street, Suite 1540
Address 2:
City: Chicago
State: IL
Zip: 60603
Phone: 312-425-9560
Phone Extension: 1274
Fax: 312-425-9564
Mobile:
Email: jcousin@infrastructure-eng.com

Project Information:

Project Description: Utility search

Are manhole/handhole installations planned in the public way?

- Yes
No

Tunneling (Includes Directional Boring) Variances Requested?

- Yes (variance to Chapter 10-20-200)
No

Project No.: P-16-3434-01

Construction Date: 08/01/2017

Project Location:

Address 1: Area bounded by and including S ASHLAND AVE, W 58TH ST, S WALLACE ST, and W 59TH ST.

Address 2: DOTMaps
https://www.cdotmap.com/ouc/project_preview#link_id=362c50a1-ea61-11e6-bccd-1b4697a30891

Additional Location Description:

The Englewood Line Nature Trail is a proposed 1.7-mile trail conversion of abandoned rail line that will provide recreation, transportation, and connectivity for residents. The City of Chicago transferred the rail line from Norfolk Southern Railway Company in 2014, and has been recommended in multiple planning processes to convert the rail line to a nature trail. This planning project will develop plans for the full development of the trail.

Project Coordinator 1: Julius Cousin III Phone:312-425-9560 Extn: 1274

Project Coordinator 2: Harish Goyal Phone:312-425-9560 Extn: 1249

Purpose of Review

Vacation/Dedication/Subdivision

Information Retrieval

Existing Facility Protection

Office of Underground Coordination Member Response

[\(Hide Comments\)](#)

Responded By	Date		No Existing Facilities	Existing Facility (Plans Attached)	Existing Facility (Narrative Attached)
Frank Duffy 01-MDE/Thermal Chicago Corporation 312-447-1600	02/08/2017		✓		
Julio Cajigas 01-Sidera Networks LLC / Lighttower (312)955-2682	02/08/2017		✓		
Joseph McCarthy 01-Bureau of Forestry (312) 746-5254	02/08/2017				✓
Comments:	The Bureau of Forestry is not involved at this time. The applicant should include a drawing(s) indicating existing trees as well as proposed landscaping for the project area on the proposed project drawings for the "Existing Facility Protection" phase of OUC review. The drawing(s) must indicate tree size in diameter measured at 4.5' above the ground and tree species. Any existing parkway tree proposed for removal must be clearly identified on the drawings. Forestry will further review for involvement at that time.				
Terrance House 01-Wide Open West LLC 630-770-4956	02/08/2017				✓
Leslie Paschal 01-ComEd - Transmission 630-437-4767	02/08/2017		✓		
Narciso Cayanan Jr. 01-CDOT Project Development (312) 744-7766	02/08/2017		✓		
Grazyna Lewandowska CTR - CDWM Water	02/08/2017				

Section Consultant 312-894-4472		0	✓
Comments:	Dimensions as shown on this drawing are approximate. Actual locations of water mains should be obtained from test holes. If test holes are desired, contact Bureau of Engineering Services - Water Section for an estimate of cost.		
Samantha Morales 01-Abovenet / Zayo Communications, Inc. (708) 699-9012	02/13/2017	✓	
Katherine Zulawski 01-Level 3 Communications (847) 471-1465	02/13/2017	✓	
Matthew Rahn 01-RCN 309-613-0689	02/13/2017	✓	
Hoise Lias Peoples Gas 312-240-4710	02/14/2017	0	✓
Comments:	See attached PGL Atlas for Facilities		
Amanullah Shaikh M.W.R.D. 312-751-3199	02/14/2017	0	✓
Brian Howard 01-CTA- Engineering 312-922-0508	02/20/2017	✓	
Craig Winfield AT&T-Illinois/SBC (708) 396-8076	02/21/2017		✓
Michel Soreze 01-ComCast 224-229-4257	02/21/2017	0	✓
Comments:	We have underground at all areas represented between the yellow line containing green dashes on the attached diagram		
Jamey Shirley 01-Sunesys, LLC 630-613-3280	02/22/2017	✓	
Antonio Bautista 01-CDOT - Division of Electrical Operations 312-746-8180	02/24/2017	0	✓
Comments:	DETAILS SHOWN IN THE ATTACHED DRAWINGS ARE APPROXIMATE AND MAY NOT BE UP-TO-DATE. UTILITIES SHOWN OR NOT SHOWN IN THE DRAWINGS MUST BE FIELD VERIFIED.		
Jesus Lopez 01-T-Mobile Central LLC (312) 405-2457	02/25/2017	✓	
Jim Todd 01-MCI 708-458-6410	02/27/2017	0	✓
Comments:	Copy of drawings are attached.		
Israel Perez 01-Department of			

Water Management - Sewer Section 312-742-7103	02/28/2017		✓
William McIntyre 01-CDOT Engineering 312-742-3219	03/08/2017	✓	
Derrek Harvey 01-CDOT-Red Light Cameras Reviewed By Xerox 312-762-0116	03/09/2017	✓	
Joseph Osowski 01-CTA - Traffic 312-681-4151	03/09/2017		✓
Comments:	CTA operates buses on 59th and on Ashland. Please contact CTA Traffic Planning to discuss impacts should there be a disruption to bus routing or bus stop locations. At least 2 weeks' notice is required for coordinated service changes. CTA point of contact for Traffic Planning South Region, is Akheel Ahmed, at aahmed@transitchicago.com or at 312-681-4180.		
John Obrien 01-ComEd - Distribution 630-437-2463	03/09/2017		✓
Bobby Akhter 01-AT&T Local Network Services (630) 719-1483	03/09/2017	✓	
Matthew Williams 01-JC Decaux 312-456-2977	03/10/2017	✓	
Vasudeva Vadali 01-Chicago Park District (312) 742-4678	03/29/2017		✓
Comments:	Moran Park has border with W. 58th St.		

OUC Project Manager Comments

Comments: MISSING UTILITY-LAKESIDE TECHNOLOGY
NOTE: ANY work in the public way and/or any excavation/penetration 12ft. or greater in private property REQUIRES an OUC Existing Facility protection (EFP) Review. Please visit our website for submittal details.
<http://www.cityofchicago.org/city/en/depts/cdot/prodrs/officeofundergroundcoordination.html>

Project Manager: LaShanda Cokley

Date: 04/04/2017

End of Transmittal & Review Form

Refresh



City of Chicago
Department of Transportation
Office of Underground Coordination
30 N. LaSalle St., Suite 310, Chicago, IL 60602
Phone# (312) 744-4828 Fax# (312) 742-3138



Transmittal & Review Form

Status: Initial Review Completed OUC File #: 2017-74660
Client Query #: 70979I Process Date: 02/03/2017
PIN Number: UUQKHQNq Response Required Date: 03/09/2017

Author:

Name: Julius Cousin III
Company: Infrastructure Engineering, Inc.
Address 1: 33 West Monroe Street, Suite 1540
Address 2:
City: Chicago
State: IL
Zip: 60603
Phone: 312-425-9560
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Fax: 312-425-9564
Mobile:
Email: jcousin@infrastructure-eng.com

Submitting Agency:

Name: Julius Cousin III
Submitting Agency: Infrastructure Engineering, Inc.
Address 1: 33 West Monroe Street, Suite 1540
Address 2:
City: Chicago
State: IL
Zip: 60603
Phone: 312-425-9560
Phone Extension: 1274
Fax: 312-425-9564
Mobile:
Email: jcousin@infrastructure-eng.com

Project Information:

Project Description: Utility search

Are manhole/handhole installations planned in the public way?

- Yes
No

Tunneling (Includes Directional Boring) Variances Requested?

- Yes (variance to Chapter 10-20-200)
No

Project No.: P-16-3434-01

Construction Date: 08/01/2017

Project Location:

Address 1: Area bounded by and including S HOYNE AVE, W 58TH ST, S RACINE AVE, and W 59TH ST.

Address 2: DOTMaps
https://www.cdotmap.com/ouc/project_preview#link_id=cc94dd14-ea61-11e6-ac9e-1b4697a30891

Additional Location Description:

Part 2- The Englewood Line Nature Trail is a proposed 1.7-mile trail conversion of abandoned rail line that will provide recreation, transportation and connectivity for residents. The City of Chicago transferred the rail line from Norfolk Southern Railway Company in 2014, and has been recommended in multiple planning processes to convert the rail line to a nature trail. This planning project will develop plans for the full development of the trail.

Project Coordinator 1: Julius Cousin III Phone:312-425-9560 Extn: 1274

Project Coordinator 2: Harish Goyal Phone:312-425-9560 Extn: 1249

Purpose of Review

Vacation/Dedication/Subdivision

Information Retrieval

Existing Facility Protection

Office of Underground Coordination Member Response

[\(Hide Comments\)](#)

Responded By	Date		No Existing Facilities	Existing Facility (Plans Attached)	Existing Facility (Narrative Attached)
Frank Duffy 01-MDE/Thermal Chicago Corporation 312-447-1600	02/08/2017		✓		
Julio Cajigas 01-Sidera Networks LLC / Lighttower (312)955-2682	02/08/2017		✓		
Joseph McCarthy 01-Bureau of Forestry (312) 746-5254	02/08/2017				✓
Comments:	The Bureau of Forestry is not involved at this time. The applicant should include a drawing(s) indicating existing trees as well as proposed landscaping for the project area on the proposed project drawings for the "Existing Facility Protection" phase of OUC review. The drawing(s) must indicate tree size in diameter measured at 4.5' above the ground and tree species. Any existing parkway tree proposed for removal must be clearly identified on the drawings. Forestry will further review for involvement at that time.				
Terrance House 01-Wide Open West LLC 630-770-4956	02/08/2017				✓
Leslie Paschal 01-ComEd - Transmission 630-437-4767	02/08/2017		✓		
Grazyna Lewandowska CTR - CDWM Water Section Consultant 312-894-4472	02/08/2017			✓	
Comments:	Dimensions as shown on this drawing are approximate. Actual locations of water mains should be obtained from test holes. If test holes are desired, contact Bureau of Engineering Services - Water Section for an estimate of cost.				

Narciso Cayanan Jr. 01-CDOT Project Development (312) 744-7766	02/08/2017		✓	
Amanullah Shaikh M.W.R.D. 312-751-3199	02/10/2017		✓	
Katherine Zulawski 01-Level 3 Communications (847) 471-1465	02/13/2017		✓	
Matthew Rahn 01-RCN 309-613-0689	02/13/2017		✓	
Samantha Morales 01-Abovenet / Zayo Communications, Inc. (708) 699-9012	02/13/2017		✓	
Brian Howard 01-CTA- Engineering 312-922-0508	02/21/2017		✓	
Hoise Lias Peoples Gas 312-240-4710 Comments:	02/21/2017	0	✓	
See attached PGL Atlas for Facilities				
Jamey Shirley 01-Sunsys, LLC 630-613-3280	02/22/2017		✓	
Jamey Shirley 01-Sunsys, LLC 630-613-3280	02/22/2017		✓	
Michel Soreze 01-ComCast 224-229-4257 Comments:	02/22/2017	0	✓	
We have underground at all areas represented between the Blue line containing green dashes on the attached diagram				
Antonio Bautista 01-CDOT - Division of Electrical Operations 312-746-8180 Comments:	02/24/2017	0	✓	
DETAILS SHOWN IN THE ATTACHED DRAWINGS ARE APPROXIMATE AND MAY NOT BE UP-TO-DATE. UTILITIES SHOWN OR NOT SHOWN IN THE DRAWINGS MUST BE FIELD VERIFIED.				
Jesus Lopez 01-T-Mobile Central LLC (312) 405-2457	02/25/2017		✓	
Jim Todd 01-MCI 708-458-6410 Comments:	02/27/2017	0	✓	
Copy of drawings are attached.				
Allison Wisniewski AT&T-Illinois/SBC	02/28/2017	0	✓	✓

708-396-8090

Comments: According to AT&T existing facility records, AT&T does have underground facilities in the Area bounded by and including S HOYNE AVE, W 58TH ST, S RACINE AVE, and W 59TH ST as shown on attached conduit prints # 196,#418,#419,#420,#966

Israel Perez
01-Department of
Water Management -02/28/2017
Sewer Section
312-742-7103



William McIntyre
01-CDOT
Engineering 03/08/2017
312-742-3219



Joseph Osowski
01-CTA - Traffic 03/08/2017
312-681-4151



Comments: CTA operates buses on 59th, Racine, Ashland and Damen. Please contact CTA Traffic Planning to discuss impacts should there be a disruption to bus routing or bus stop locations. At least 2 weeks' notice is required for coordinated service changes. CTA point of contact for Traffic Planning South Region, is Akheel Ahmed, at aahmed@transitchicago.com or at 312-681-4180.

Derrek Harvey
01-CDOT-Red Light
Cameras Reviewed 03/09/2017
By Xerox
312-762-0116



John Obrien
01-ComEd -
Distribution 03/09/2017
630-437-2463



Bobby Akhter
01-AT&T Local
Network Services 03/09/2017
(630) 719-1483



Matthew Williams
01-JC Decaux 03/10/2017
312-456-2977



Vasudeva Vadali
01-Chicago Park
District 03/29/2017
(312) 742-4678



Comments: Lindbloom Park has border with W. 59th St., and Hermitage Park is within the project limits. Lindbloom Park's sewer service pipeline is connected to the City's sewer main in W. 59th St., (approx. 264' West from the West curb line of S. Damen Ave.). Hermitage Park's water service pipelines are connected to the City's water mains in S. Paulina St., (approx. 135' North from the North curb line of W. 59th St); and in W. 59th St, (approx. 183' East from the East curb line of S. Wood St.); and park's sewer service pipelines are connected to the City's sewer mains in S. Wood St,(approx. 200' North from the North curb line of W. 59th St), and in W. 59th St,(approx. 118' West from the West curb line of S. Paulina St). Any work within the Chicago Park District's property limits requires a separate permit from the Chicago Park District.

OUC Project Manager Comments

Comments: MISSING UTILITY-LAKESIDE TECHNOLOGY
NOTE: ANY work in the public way and/or any excavation/penetration 12ft. or greater in private property REQUIRES an OUC Existing Facility protection (EFP) Review. Please visit our website for submittal details.
<http://www.cityofchicago.org/city/en/depts/cdot/prodrs/officeofundergroundcoordination.html>

Project Manager: LaShanda Cokley

Date: 04/04/2017

End of Transmittal & Review Form

APPENDIX B

FHWA / IDOT Coordination

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



CHICAGO DEPARTMENT OF TRANSPORTATION
CHICAGO DEPARTMENT OF PLANNING AND DEVELOPMENT

Bridge Conditions Assessment of the Englewood Connector Railroad



IDOT Presentation

Tuesday November 24, 2015



PROJECT BACKGROUND

The Department of Planning and Development is exchanging properties between the City and the Norfolk Southern Corporation (NS). As part of that transaction, the NS has granted right of access to the City to perform bridge condition assessment reports along the NS Englewood Connector line, which is a 2 mile elevated abandoned rail line. It is the intent that the rail line may be re-purposed for public access and a potential recreational trail.

PROJECT DESCRIPTION

PROJECT LOCATION

- Englewood Neighborhood, north of 59th Street, between Hoyne Avenue and Wallace Avenue

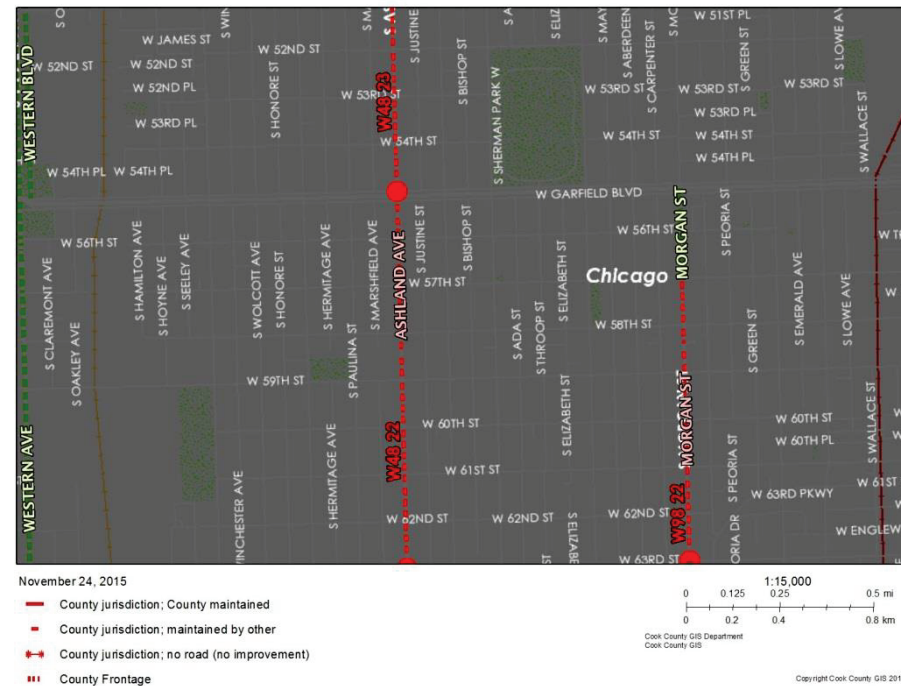
PROJECT LENGTH

- 1.7 miles oriented east-west

PROJECT OBJECTIVE

- Structural assessment of twenty-six (26) viaduct structures that previously carried a single track rail line over City streets
- Structural assessment of twelve (12) concrete retaining walls adjacent to the abandoned track supporting the track embankment.

Cook County Jurisdictional Map



CORRIDOR HISTORY

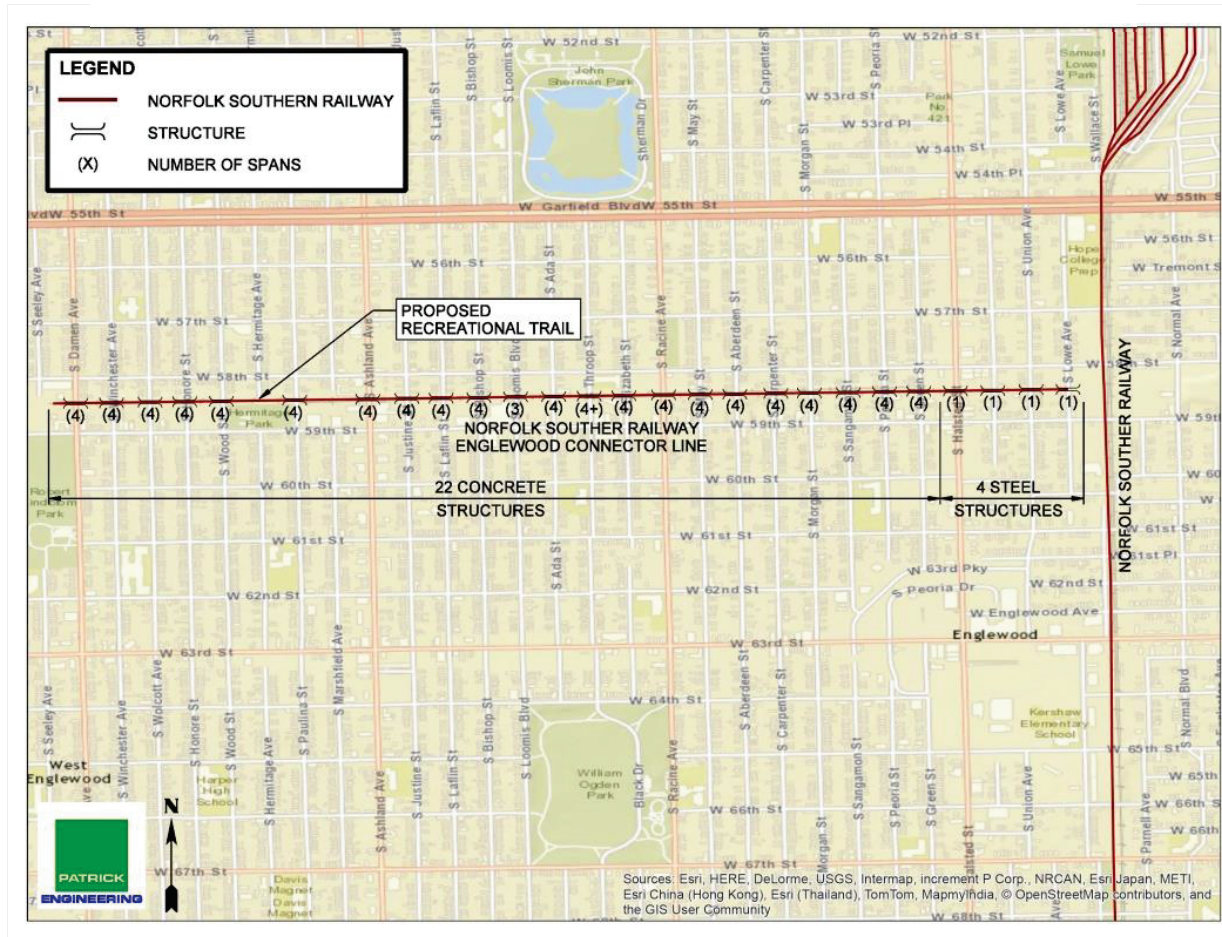
- Railroad viaduct structures built in 1917 by the Pennsylvania Railroad
- Railroad use of the single track ended in the 1970s
- Currently owned by Norfolk Southern Corporation
- All Streets under the viaducts are maintained by the City of Chicago
- Vertical clearance under the viaducts ranges from 11'-2" (Lowe Avenue and Union Avenue) to 13'-6" (Laflin Street)



INFRASTRUCTURE
ENGINEERING | INCORPORATED

PATRICK

PROJECT LOCATION MAP



**INFRASTRUCTURE
ENGINEERING** | INCORPORATED

PATRICK

TYPICAL CONCRETE VIADUCT (ELEVATION)



TYPICAL CONCRETE VIADUCT (ABOVE)



TYPICAL STEEL VIADUCT (ELEVATION)



INFRASTRUCTURE
ENGINEERING | INCORPORATED

PATRICK

TYPICAL STEEL VIADUCT (ABOVE)



TYPICAL RETAINING WALL



SCOPE OF WORK

- Bridge and retaining wall inspection
- IDOT Bridge Inspection Report (MI) Form
- Abbreviated Bridge Condition Reports

OTHER PROJECT ELEMENTS PERFORMED

- Lead based paint testing
- Corridor aerial mosaics
- Pick-up survey as necessary



INFRASTRUCTURE
ENGINEERING | INCORPORATED

PATRICK

GENERAL INSPECTION FINDINGS

- Viaducts overall are in fair condition
- Viaducts' current conditions present no immediate safety concern

PROPOSED SCOPE OF BRIDGE WORK

- Structural Repair of Concrete
- Apply Concrete Sealant
- Steel Repairs/Replacement
- Cleaning and Painting Steel

PROJECT SCHEDULE

- Notice to Proceed - July 9, 2015
- Field Inspections - August 2015
- Draft Abbreviated Bridge Condition Reports submitted to CDOT - November 2015
- Submittal of Final Deliverables - TBD



MEETING ATTENDANCE RECORD

Project Name: Conditions Assessment of the Englewood Connector Railroad Bridges **Date:** November 24, 2015

Patrick Project Number / Task: 21577.401 **Meeting Location:** CDOT; Conference Room 4A

<u>NAME</u>	<u>COMPANY</u>	<u>PHONE / EXT.</u>	<u>E-MAIL ADDRESS</u>
Luis Benitez <i>YOB</i>	Department of Transportation	(312) 744-5807	Luis.Benitez@cityofchicago.org
Moira Coughlin	Department of Transportation	(312) 744-9458	Moira.Coughlin@cityofchicago.org
<i>NC</i> Nelson Chueng	Department of Planning and Development	(312) 744-5756	Nelson.Chueng@cityofchicago.org
<i>SLC</i> Sarah Czaplicki	Patrick Engineering	(630) 795-7318	sczaplicki@patrickco.com
<i>SRL</i> Steve Lynch	Patrick Engineering	(312) 201-7951	slynch@patrickco.com
Ken Smorynski	Infrastructure Engineering	(312) 425-9560 x1260	KSmorynski@infrastructure-eng.com
Pankaj Kumar	Infrastructure Engineering	(312) 425-9560 x1240	pkumar@infrastructure-eng.com
<i>ZH</i> Zubair Haider	Illinois Department of Transportation		Zubair.Haider@illinois.gov
<i>PHONE</i> James Skvarla	Illinois Department of Transportation		James.Skvarla@illinois.gov

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 08
Wednesday, January 20, 2016, 1:00 pm to 2:30 pm
Attendees: see attached sign in sheet

Meeting Items:

1. Abbreviated Bridge Condition Report (ABCR)

CDOT provided comments regarding the 2 ABCRs previously provided (Elizabeth Street and Lowe Avenue) prior to start of conference call with IDOT.

CDOT's Comments:

Regarding Lowe Avenue:

- CDOT takes no exception to the level of detail in the ABCR.
- Confirm with IDOT on what size location map is required (During call with IDOT, confirmed that no additional location map is needed.
- The expected service life of the structure is to be adjusted to 50 years since the structure will not be exposed to salt, only experience pedestrian loading, pedestrian loading will not cause fatigue.
- The cost should be revised for the replacement to more accurately reflect the true replacement cost. Replacement cost included only shows cost for a new superstructure. However if the structure needs to be raised 4-6 ft to meet IDOT pedestrian bridge clearance requirements 17'-3", new abutments would be required and adjustment of the trail profile would be necessary. The revised cost will change the recommended structure to rehabilitation instead of reconstruct.
- Need to add a railing if the rehabilitation recommendation is carried forward. Currently not included.

IDOT's Comments

- All ABCR(s) require a structure number to be included in order to be reviewed.
- All new/reconstructed pedestrian bridges must have a vertical clearance of 17'-3".
- Rehabilitation of the existing structure and keeping the existing vertical clearance at 11'-2" will require a waiver from local roads.

2. General Project Information

Project will utilize local funding for the Phase I.

Project Termini have not yet been confirmed. CDOT is working to establish the termini.

ABCR needed for all walls greater than 10' from top of wall to bottom of footing (Local Roads criteria) or with an exposed wall height greater than 7' (Bridge manual).

3. Project Deliverable

The final deliverable for the Task Order will be a complete Project Development Report (PDR). This will complete the federal requirements for the Phase I and allow the project to be eligible for federal funding for the Phase II, Phase III and construction.

No aerial mosaics will be required. Muller & Muller (WBE) to be reassigned to help with the PDR in some capacity.

4. Survey

The City indicated that the survey to be provided is not completed.

5. Action Items

Ownership	Action Item	Status	Anticipated Completion Date
CDOT	Provide corridor survey	In Progress	09/10/2015
Patrick	Provide revised draft ABCR for CDOT to forward to IDOT	Completed.	12/18/2015
CDOT	Provide guidance on what information CDOT wants to see displayed on the mosaics	In Progress	10/01/2015
CDOT	Provide bid tabs and plan set from Bloomingdale Trail project for use in the cost estimate.	Completed	10/22/2015
Patrick	Prepare Power Point presentation for IDOT Kick-off meeting	Completed	11/24/2015
Patrick	Prepare level of effort summary documenting from where things stand currently, what it would take to complete the Phase I PDR for the full 1.7 mile stretch.	In progress	02/04/2016



MEETING ATTENDANCE RECORD

Project Name: Conditions Assessment of the Englewood Connector Railroad Bridges **Date:** January 20, 2016

Patrick Project Number / Task: 21577.401 **Meeting Location:** CDOT; Conference Room 4A

<u>NAME</u>	<u>COMPANY</u>	<u>PHONE / EXT.</u>	<u>E-MAIL ADDRESS</u>
Luis Benitez <i>LB</i>	Department of Transportation	(312) 744-5807	Luis.Benitez@cityofchicago.org
Moira Coughlin	Department of Transportation	(312) 744-9458	Moira.Coughlin@cityofchicago.org
<i>NC</i> Nelson Chueng	Department of Planning and Development	(312) 744-5756	Nelson.Chueng@cityofchicago.org
<i>SLC</i> Sarah Czaplicki	Patrick Engineering	(630) 795-7318	sczaplicki@patrickco.com
<i>SRL</i> Steve Lynch	Patrick Engineering	(312) 201-7951	slynch@patrickco.com
Ken Smorynski	Infrastructure Engineering	(312) 425-9560 x1260	KSmorynski@infrastructure-eng.com
Pankaj Kumar	Infrastructure Engineering	(312) 425-9560 x1240	pkumar@infrastructure-eng.com
<i>PHONE</i> Zubair Haider	Illinois Department of Transportation		Zubair.Haider@illinois.gov
<i>PHONE</i> James Skvarla	Illinois Department of Transportation		James.Skvarla@illinois.gov
<i>PHONE</i> Jim Klein	Illinois Department of Transportation		James.Klein@illinois.gov
<i>PHONE</i> Matt Humke	Illinois Department of Transportation		Matt.Humke@Illinois.gov

Design Policy: 3R Rural Urban Bicycle Guidelines

Proposed Cross Section:

Location	Surface Width	Type	Roadway Width	Shoulder Type Curb & Gutter Type
	12-ft	Bituminou	N/A	5' landscaped

Bridge No.: Rehab only (f-f Curb): No change (f-f of Rail): _____

Required Structural TSL BLR 10210 BCR Other

Any proposed traffic signals ? : Yes No Location(s): _____

Are signal warrants met ? : Yes No

Variances:

Item Description	Standard	Justification
Vertical Clearance	BLR 30-3.1 - 14'-0"	Property Impacts

Maintenance of Traffic/Detour: Bridge rehabilitation work will be done under staged construction to maintain local traffic on the roadways. Trail construction will be staged linearly along within the trail row to minimize impacts to local traffic

Level of Environmental Significance:

Categorical Exclusion: Does the Project meet the following:

- Actions which do not involve potential for extraordinary circumstances.
- Actions which involve potential for extraordinary circumstances requiring concurrence from the FHWA because they involve one or more of the following:
 - (a) Involve in-stream work requiring non-routine mitigation
 - (b) Require an individual section 404 permit from the Corps of Engineers
 - (c) Require additional right-of-way (including temporary or permanent grading easements) which involves relocation of residences or businesses or exceeds one or more of the following:
 - (1) Ten acres of prime farmland
 - (2) Three acres of land per mile of roadway or 10 acre total for a non-linear (spot) improvement (e.g., bridge, intersection)
 - (3) Land takes from 10 property owners
 - (d) Require substantial changes in access control
 - (e) Substantially increase 100-year flood water surface elevations
 - (f) Involve impacts on wetlands
 - (g) Require preparation of a biological assessment for threatened or endangered species

- (h) Affect the characteristics that would qualify an historic or archaeological resource for inclusion on the National Register of Historic Places
- (i) Take land (permanent or temporary use) from section 106 resource, or
- (j) Take land (permanent or temporary use) from section 4(f) resource, or
- (k) Be controversial on environmental grounds or inconsistent with federal, state or local laws relating to the environment

Procedures in addition to above factors:

- Environmental Class of Action Determination (ECAD)

Additional Information:

Short list of project specific items not covered above that may affect the schedule, approval or implementation of the project.

None

Attachments:

Functional Classification Map with project limits

FHWA/IDOT/City of Chicago
Monthly Coordination Meeting
Project Introduction for the
Englewood Trail, Hoyne Avenue to Wallace Avenue
Section No. 16-E5482-00-BT

June 12, 2017

This was the first presentation of this project. The purpose of the presentation was to provide an overview of the project and discuss the project's scope, termini, and type of processing.

The project consists of the conversion of an abandoned elevated Norfolk Southern (NS) rail line to a public recreational trail. The project length is approximately 1.75 miles. Rail Service was discontinued on this line in the early 1960's and the rails have since been removed. The railway ties and ballast have overgrown with grass and vegetation. The existing ROW corridor is 60' wide corridor with additional adjacent properties owned by NS. All NS property through the corridor is in the process of being transferred to the City of Chicago, independent of this project. The railway is elevated approximately 15' to 17' above street level. There are 26 bridges crossing over City streets, with vertical clearances ranging from 12' to 13'. There are no IDOT maintained roadways within the project limits. There are currently no access ramps and no lighting of the proposed trail.

The anticipated proposed improvement consists of constructing a 12' wide bituminous multi-use path with 5' wide grass shoulders (6:1 or flatter) for the length of the corridor. All access ramps will be 10' wide and ADA compliant (5% grade or flatter) and will be located approximately every ¼-mile. Emergency vehicle access will only be provided at the access ramps at Hoyne Avenue and Halsted Street and these two ramps will be 12' wide. LED lighting is also proposed for the project area.

In general, the flat area along the corridor ranges from 22' to 27' wide. Outside of that, there are 3:1 slopes down to the street level. Continuous railing along the path is not proposed, as the available clear zones meet the AASHTO bicycle design guidelines. Railings will be provided atop all bridges and the need for end treatments for these bridge railings will be investigated.

The Bridge and Retaining Wall Condition Reports were completed in the fall of 2016 and are currently under final review by IDOT Central Office Bureau of Bridges & Structures. The preliminary geometrics (trail and access ramp alignments) were recently reviewed and approved by CDOT. The proposed drainage plan is in process and the Project Development Report is being prepared.

The Environmental Survey Request (ESR) Form was submitted to IDOT in February 2017. Special Waste Phase I & II Environmental Site Assessment Reports were completed for the study area in June 2015. The cultural clearance was received on 4/24/2017. Natural resource clearance is still pending.

Approximately 1.8 acres of proposed ROW is needed for this project from four parcels adjacent to the corridor, primarily to accommodate the access ramps. One parcel (Hermitage Park) is publicly owned by the Chicago Park District (CPD) and 0.3 acres of land is needed for this project. A meeting with the CPD was held on May 8, 2017 to discuss the project and at this meeting, the CPD noted that they are in favor of the project. IDOT and FHWA determined that the project would not use a Section 4(f) property as defined in 23 CFR 774.13(g), as long as the CPD would be a co-sponsor of the project and a letter is received from the CPD noting that they are in agreement with the project and the proposed use of their land. This letter will be included in the Project Development Report.

The only potential design exception for this project would be for the vertical clearance of the roadways beneath the trail bridges. IDOT recommended submitting the BLR 22210 form for this exception, and to note that this is an existing condition, all work on this project will be above the bridge deck level, and it is not in the scope of the project to raise any of the bridges or lower any roadways below the elevated trail, and they would be in support of this exception.

IDOT asked if this project will be similar to the Bloomingdale Trail. CDOT responded that it will be similar, in that it will be a public elevated trail, however, it will not include as much hardscape reconstruction and side parks as the Bloomingdale Trail. The Englewood Trail will be designed to utilize as much of the existing landscape along the corridor as possible.

There has been about a year and half of community involvement thus far on this project. The City of Chicago has engaged Teska Associates to do the initial planning work and community outreach. Greater Englewood is also a local non-profit organization who has assisted Teska in this effort. There have been walking tours, church meetings, aldermanic coordination and community meetings held to date. There will also be a Public Meeting held as part of the Phase I Study anticipated to be scheduled for mid-July 2017. IDOT noted that the aforementioned public involvement plan is acceptable, as long as the property owners affected by ROW acquisition are sent certified letters explaining the project and inviting them to the Public Meeting.

IDOT and the FHWA concurred with the scope and termini of the project. The design criteria would be BLR Manual Chapter 42 (Bicycle Facilities). The construction cost estimate for the project is \$35 mil. CMAP Transportation Alternatives Program (TAP) funds have been applied for, which could cover 80% of the Phase II and construction project costs. Phase I is currently being locally funded by CDOT. The current construction target is 2020. Given that the construction cost is above \$31 mil and more than 15% of the funds are likely to come from federal sources, the project processing needs to be a Federally-approved CE Report.

Steve Lynch, Patrick Engineering/Moria Kent, CDOT

ATTENDANCE ROSTER BUREAU OF LOCAL ROADS AND STREETS

Project / Topic: FHWA/IDOT/City of Chicago Coordination Meeting

Date: June 12, 2017

Time: 1:00 PM

Location: IDOT District 1

Room: Executive Conference Room

	Name (Please Print)	Representing	Phone Number	Email Address
1	Chris Byars <i>WCB</i>	FHWA	312-886-1606	chris.byars@dot.gov
2	Chris Holt <i>CH</i>	IDOT, D1-BLRS	847-705-4201	christopher.holt@illinois.gov
3	William Raffensperger <i>WR</i>	IDOT, Central BLRS	217-785-1676	william.raffensperger@illinois.gov
4	Omar Qudus <i>OQ</i>	FHWA	217-492-4634	omar.qudus@dot.gov
5	Zubair Haider <i>ZH</i>	IDOT, D1-BLRS	847-705-4206	zubair.haider@illinois.gov
6	Jason Salley	IDOT, D1 Programming	847-705-4085	jason.salley@illinois.gov
7	Dan Burke <i>DB</i>	CDOT, Engineering	312-744-5807	dan.burke@cityofchicago.org
8	Jeffrey Sriver <i>(on phone)</i>	CDOT, Project Development	312-744-7080	jeffrey.sriver@cityofchicago.org
9	John Sadler <i>JS</i>	CDOT, Engineering	312-744-0488	john.sadler@cityofchicago.org
10	Moira Kent <i>MR</i>	CDOT, Engineering	312-744-9458	moira.kent@cityofchicago.org
11	Alaina Bridges <i>AB</i>	CDOT, Project Development	312-744-3607	alaina.bridges@cityofchicago.org
12	Grant Davis <i>GD</i>	CDOT, Project Development	312-744-3528	grant.davis@cityofchicago.org
13	Luis Benitez <i>LB</i>	CDOT, Engineering	312-744-5807	luis.benitez@cityofchicago.org
14	James Skvarla <i>JS</i>	IDOT, D1-BLRS (Consultant)	847-705-4021	james.skvarla@illinois.gov
15	Scott Stitt	IDOT, Central BDE	217-785-0721	scott.stitt@illinois.gov
16	John Sherrill <i>JSS</i>	IDOT, Springfield	217-785-4181	john.sherrill@illinois.gov
17	Mary Ellen Mack <i>MEM</i>	IDOT, D 1-Construction	847-705-4465	maryellen.mack@illinois.gov
18	Temi Latinwo <i>TL</i>	IDOT, D1-BLRS	847-705-4179	temi.latinwo@illinois.gov
19	Michael Kowalczyk <i>MAR</i>	FHWA	312-886-1604	michael.kowalczyk@dot.gov
20	Abraham Emmanuel	CDOT, Traffic Safety	312-742-0804	aemmanuel.@cityofchicago.org
21	Nathan Roseberry <i>NR</i>	CDOT, Engineering	312-744-5936	nathan.roseberry@cityofchicago.org
22	JARROD CEBULSKI	PATRICK ENGINEERING	630-795-7468	JCEBULSKI@PATRICKCO.COM
23	Steve Lynch	Patrick Eng.	312 201-7951	slynch@patrickco.com
24				
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APPENDIX C

Public Meeting Documentation

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



**Chicago Department of Transportation and
Department of Planning & Development
CDOT Project: E-5-482**

**Englewood Elevated Railroad Connector at 59th Street
(from Hoyne to Lowe)**

Summary of Public Meeting

July 13, 2017

Englewood Elevated Railroad Connector at 59th Street (from Hoyne to Lowe) Summary of Public Meeting

An open house Public Meeting was held on July 13, 2017 for the Englewood Elevated Railroad Connector at 59th Street Phase I Study at the Charles R. Henderson Elementary School (5650 S. Wolcott Avenue). Approximately thirty-five (35) people attended this meeting, at which exhibit boards showing the proposed improvement were displayed in addition to other displays. The exhibit boards included a location map, existing overview map, segment area enlargements and sections, conceptual rendering, typical sections and project schedule. A PowerPoint presentation describing the history of the project and other previous studies ran continuously in an area adjacent to the exhibit boards.

Staff members from the City of Chicago Department of Transportation (CDOT), City of Chicago Department of Planning and Development (DPD), Teska Associates, Inc., Grow Greater Englewood and Patrick Engineering were in attendance to answer questions from stakeholders and the general public. Meeting announcements were placed in the Chicago Sun-Times newspapers prior to the Public Meeting on 06/28/2017 and again on 07/10/2017. Certified mail letters were sent to the five (5) private property owners whose properties will be impacted by the project. The meeting location was coordinated in advance with the local Alderman. Alderman Raymond Lopez (15th Ward), Alderman Toni Foulkes (16th Ward) and members of their staffs attended the meeting.

Project informational brochures were distributed throughout the community prior to the meeting and were available at the meeting for all attendees, and all meeting participants were offered a comment sheet to fill out at the meeting or to submit at a later date via mail or email to DPD.

Summary of Public Meeting Comments

Of the 33 attendees, 11 submitted written comments regarding the proposed improvement. No additional comments were submitted through the mail or by email subsequent to the meeting within the two-week open comment period of the Public Meeting. A considerable amount of comments were in support of the project. Other comments expressed concern towards the local community's role in the final design of the project and the potential gentrification of the neighborhood post construction of the project.

**Englewood Elevated Railroad Connector at 59th Street (from Hoyne to Lowe)
Summary of Public Meeting**

BROCHURE

PUBLIC INFORMATION MEETING

SCHEDULED BY CITY OF CHICAGO DEPARTMENT OF TRANSPORTATION AND DEPARTMENT OF PLANNING & DEVELOPMENT FOR IMPROVEMENT OF THE 59TH STREET LINE BETWEEN HOYNE AVENUE AND LOWE AVENUE IN ENGLEWOOD

PROVIDE YOUR COMMENTS AT AN OPEN HOUSE BETWEEN: 5:30-7:00 P.M.

THURSDAY, JULY 13, 2017

CHARLES R. HENDERSON ELEMENTARY SCHOOL,
5650 S. WOLCOTT AVENUE,
CHICAGO, IL 60636

The Chicago Department of Transportation (CDOT) and Department of Planning & Development (DPD) will host an Open House Public Meeting concerning the proposed improvement of the 59th Street Line between Hoyne Avenue and Lowe Avenue for the purpose of creating a multi-use trail along the former elevated rail line. The line will be the first phase of a vision for a trail loop that will connect the 49th and 59th abandoned rail lines through the neighborhoods of Englewood, West Englewood and Back of the Yards.

The meeting room is accessible to persons with disabilities. Anyone needing special accommodations should inform one of the contact people below at least five days prior to the meeting. Refreshments will be provided.



FOR MORE INFORMATION, CONTACT:

MOIRA KENT
CHICAGO DEPT. OF TRANSPORTATION
PROJECT MANAGER
(312) 744-9458
Moira.Kent@cityofchicago.org

NELSON CHUENG
CHICAGO DEPT. OF PLANNING & DEVELOPMENT
PROJECT MANAGER
(312) 744-5756
Nelson.Chueng@cityofchicago.org

TAKE THE SURVEY & LEARN MORE AT: www.engagewoodline.org

**Englewood Elevated Railroad Connector at 59th Street (from Hoyne to Lowe)
Summary of Public Meeting**

NEWSPAPER ADVERTISEMENT

CLASSIFIEDS 312.321.2345

Client Name: PATRICK ENGINEERING INC.
Advertiser: Main/049/
Section/Page/Zone: Charles R Henderson Elementary
Description:
B&W
1031000-01
Insertion Number:
Size: 2x19.00
Color Type: B&W
Ad Number:
Color Type: B&W

CHICAGO SUN-TIMES
Publication Date: 07/10/2017
This E-Sheet(s) is provided as conclusive evidence that the ad appeared in the Chicago Sun-Times on the date & page indicated. You may not create derivative works, or in any way exploit or repurpose any content.

Public Notices
PUBLIC BUILDING COMMISSION OF CHICAGO
Room 200
Richard J. Daley Center
50 West Washington Street
Chicago, IL 60602
312-744-3090
www.pbccchicago.com
ADVERTISEMENT FOR QUALIFICATIONS
Request for Qualifications (RFQ) will be received by the Public Building Commission of Chicago (PBC) before the date and time (Chicago time) listed below.
PROJECT NAME:
The Public Building Commission of Chicago is soliciting a Request for Qualifications (including firms proposing to have a controlling interest in Joint Ventures) for the following project:
PREQUALIFICATION OF GENERAL CONTRACTORS FOR THE CHICAGO PARK DISTRICT CAPITAL IMPROVEMENT PROGRAM PROJECTS
ISSUE DATE: JUNE 30, 2017
SUBMISSION DUE DATE:
Request for Qualifications submittals must be delivered by July 21, 2017 at 4:00PM (CT) to the attention of Patricia Montenegro, Public Building Commission of Chicago, Richard J. Daley Center, 50 W. Washington Street, Room 200, Chicago, IL 60602
CONTACT:
Patricia Montenegro, Contract Officer; patricia.montenegro@cityofchicago.org
DOCUMENT AVAILABLE:
website: www.pbccchicago.com or Public Building Commission of Chicago, Richard J. Daley Center, Front Receptionist, 50 W. Washington Street, Room 200, Chicago, Illinois 60602
Mayor Rahm Emanuel Chairman
Carina E. Sánchez Executive Director

Public Notices
PUBLIC BUILDING COMMISSION OF CHICAGO
Room 200
Richard J. Daley Center
50 West Washington Street
Chicago, IL 60602
312-744-3090
www.pbccchicago.com
ADVERTISEMENT FOR PROPOSALS
Request for Proposals (RFP) will be received by the Public Building Commission of Chicago (PBC) before the date and time (Central Standard Time) listed below.
PROJECT NAME:
The Public Building Commission of Chicago is issuing a Request for Proposals (including firms proposing to have a controlling interest in Joint Ventures) for the following project:
DESIGN-BUILD SERVICES FOR THE SOUTH SIDE HIGH SCHOOL (PS2096)
ISSUE DATE: JUNE 30, 2017
PBC Project No. 05185
South Side High School
Normal and 69th St., Chicago, IL
The Project will include site preparation; design and construction of the South Side High School; and associated site development work in general accordance with the Chicago Public School's program and building guidelines.
SUBMISSION DUE DATE:
Request for Proposals submittals must be delivered by July 31, 2017 at 11:00AM (CST) to the attention of Raven A. DeVaughn, Public Building Commission of Chicago, Richard J. Daley Center, 50 W. Washington Street, Room 200, Chicago, IL 60602
CONTACT:
Raven A. DeVaughn, Contract Officer; raven.devaughn@cityofchicago.org
DOCUMENT AVAILABLE:
website: www.pbccchicago.com or Public Building Commission of Chicago, Richard J. Daley Center, Front Receptionist, 50 W. Washington Street, Room 200, Chicago, Illinois 60602
Mayor Rahm Emanuel Chairman
Carina E. Sánchez Executive Director

Storage - Legal
NOTICE OF PUBLIC SALE OF PERSONAL PROPERTY
Notice is hereby given that pursuant to Section 4 of the Self-Service Storage Facility Act, State of Illinois, that Public Storage / PS Orangoeco, Inc. will conduct sale(s) at Public Storage by Competitive bidding on July 25th, 2017 at 10:00 am on the Premises where property has been stored, which are located at Public Storage#-20485, 141 S. Wabash Chicago, IL 60605, (312) 427-1043. In the matters of the personal property for the individuals listed below:
Unit - Customer 0063 - Noble, Steven B1019 - GSSP Enterprise B1035 - Muhammad, Diana B1043 - Millan, Jacob B1051 - McCarroll, Lester B1062 - Turner, Tma C2006 - Powell, Melony C2056 - Morris, Marcus C2064 - Dukes, Sheila D3048 - Peyton, Indrani E4202 - Robbins, Jacqueline E4034 - Brooks, Akosua E4113 - Burgin, Shatara F5012 - Ghee, Marquis F5013 - Gbolade, Owode F5027 - Otugo, Chijioke F5037 - Hemphill, Temple F5052 - Zobeil, Thomas F5145 - Redwood, Mark H7011 - Payne, Lori H7055 - Drape, Shunae H7075 - Schultz, John H7085 - Turner, Dominique H7115 - Sturges, Willie
Public Storage/ PS Orangoeco, Inc. Purchases must be made with cash only and paid at the time of sale. All goods are sold as is and must be removed at the time of purchase. Sale is subjected to adjournment.
7/10, 7/17/17 #1032067
NOTICE OF PUBLIC SALE OF PERSONAL PROPERTY
Notice is hereby given that pursuant to Section 4 of the Self-Service Storage Facility Act, State of Illinois, that Public Storage / PS Orangoeco, Inc. will conduct sale(s) at Public Storage by Competitive bidding on July 25th, 2017 at 11:00 am on the Premises where property has been stored, which are located at Public Storage#-08067, 1711 West Fullerton Ave, Chicago, IL 60614, (773) 525-5518. In the matters of the personal property for the individuals listed below:
Unit - Customer 1033 - Sutton, Ella I109 - barnett, Jeffrey I150 Shatara F5012 - Ghee, Marquis F5013 - Gbolade, Owode F5027 - Otugo, Chijioke F5037 - Hemphill, Temple F5052 - Zobeil, Thomas F5145 - Redwood, Mark H7011 - Payne, Lori H7055 - Drape, Shunae H7075 - Schultz, John H7085 - Turner, Dominique H7115 - Sturges, Willie
Public Storage/ PS Orangoeco, Inc. Purchases must be made with cash only and paid at the time of sale. All goods are sold as is and must be removed at the time of purchase. Sale is subjected to adjournment.
7/10, 7/17/17 #1032067

Storage - Legal
Notice of Lien Sale / Public Auction
In accordance with the provisions of State law, there being due and unpaid charges for which the undersigned is entitled to satisfy an owner and/or manager's lien of the goods hereinafter described and stored at the Life Storage location(s) listed below. And, due notice having been given, to the owner of said property and all parties known to claim an interest therein, and the time specified in such notice for payment of such having expired, the goods will be sold at public auction at the below stated location(s) to the highest bidder or otherwise disposed of on Tuesday, July 25th, 2017 at 09:00 am in the order below. #426, 615 W Pershing Rd Chicago IL 60609 Ph 773.376.5167: 106 Gloria Williams, 107 Gloria Williams, 217 Elijah Paige, 452 William Patterson #551, 2261 S State St Chicago IL 60616 Ph 312.842.0004: 210 Shaun Bramm, 25 Quiana Jenkins 1011 Dominique King, 2020 Jeanne Williams, 2091 Kenneth Fisher, 2142 Renaldo Fuller, 2182 Kamron Lowe, 3076 Tracey Everett-Carter, 3077 Gabrielle Ware, 3085 Theresa Kearns, 4028 Michelle Pitts, 4038 Thelma Brown, 4038 Sharon Brown, 4135 Ningfeng Zhang, 4145 John H. Reed, 4150 Craig Wilson, 5080 Cornelius Bass. #878, 1205 W Jackson Blvd Chicago IL 60607 Ph 312.421.0001: 4135 Mario Ratcliff, 4173 Melanie M. Davis, 5019 Jessica Dole, 5031 Hariv Harris #877, 333 W Ohio St Chicago IL 60654 Ph 312.222.0136: 4025 Tajiana Dragovic, 4089 Darrin Lewis. Pub: 7/3 & 7/10/2017 1030831
Public Hearings
City of Chicago, Department of Transportation
Improvement of the 59th Street Line Trail Between Hoyne Avenue and Lowe Avenue
Public Involvement Meeting
The Chicago Department of Transportation (CDOT) and Department of Planning & Development (DPD) will host an Open House Public Meeting concerning the proposed improvement of the 59th Street Line Trail between Hoyne Avenue and Lowe Avenue for the purpose of creating a multi-use trail along the former elevated rail line. The Trail will be the first phase of a vision for a trail route that will connect the 59th abandoned rail lines through the neighborhoods of Englewood, West Englewood and Back of the Yards.
Date: Thursday, July 13, 2017
Time: 5:30 PM to 7:00 PM
Location: Charles R Henderson Elementary School
5540 S. Wolcott Avenue
Chicago, IL 60636
The meeting will be conducted in an informal, open house format and interested persons may attend at any time between 5:30 pm and 7:00 pm. Information available for review during the meeting will include preliminary reports and an engineering analysis with drawings, maps and aerial photography and a tentative construction schedule. The public will have an opportunity to review exhibits, provide comments and discuss their concerns directly with CDOT and DPD representatives. The meeting room is accessible to persons with disabilities. Anyone needing special accommodations should inform one of the contact people below at least five days prior to the meeting.
For more information, contact:
Moira Kent, Chicago Dept. of Transportation, (312) 744-9458, Moira.Kent@cityofchicago.org
Nelson Chueng, Chicago Dept. of Planning & Development, (312) 744-5756, Nelson.Chueng@cityofchicago.org
Pub: 6/28 & 7/10/2017 1031000

Storage - Legal
NOTICE OF PUBLIC SALE OF PERSONAL PROPERTY
matters of the personal property for the individuals listed below:
Unit - Customer A171 - Nashert, Kyle A174 - Barrett, Allen A211 - Herbert, Nicole A310 - Howe, Kathryn A435 - Moore, Sarah A491 - Dominguez, Michael A504 - Stanback, sheila C023 - Castellanos, Carlos Randy C024 - Goins, David C060 - Kelly, Robert C064 - CAVALLERO, JANET D599 - Wojcik, Thomas D650 - Lapp, Richard D707 - Busk, Margaret D724 - Halpin, Malory
Public Storage/ PS Orangoeco, Inc. Purchases must be made with cash only and paid at the time of sale.
All goods are sold as is and must be removed at the time of purchase. Sale is subjected to adjournment.
7/10, 7/17/17 #1032070
NOTICE OF PUBLIC SALE OF PERSONAL PROPERTY
Notice is hereby given that pursuant to Section 4 of the Self-Service Storage Facility Act, State of Illinois, that Public Storage / PS Orangoeco, Inc. will conduct sale(s) at Public Storage by Competitive bidding on July 25th, 2017 at 10:00 am on the Premises where property has been stored, which are located at Public Storage #08066, 947 West Van Buren, Chicago, IL 60607, (312) 421-0049. In the matters of the personal property for the individuals listed below:
Unit - Customer 0114 - Walsh, Daryl 0126 - Hiegel, ADRIENNE 0130 - Pennington, Paris 0410 - Lathan, Kris 0416 - Hibbler, Andrea 0539 - Turner, Franklin 0567 - Reed, Lawrence 0720 - Fowler, Anais 0763 - Collozo, Emilio
Public Storage/ PS Orangoeco, Inc. Purchases must be made with cash only and paid at the time of sale.
All goods are sold as is and must be removed at the time of purchase. Sale is subjected to adjournment.
7/10, 7/17/17 #1032067
NOTICE OF PUBLIC SALE OF PERSONAL PROPERTY
Notice is hereby given that pursuant to Section 4 of the Self-Service Storage Facility Act, State of Illinois, that Public Storage / PS Orangoeco, Inc. will conduct sale(s) at Public Storage by Competitive bidding on July 25th, 2017 at 10:00 am on the Premises where property has been stored, which are located at Public Storage #21807, 1916 N. Elston Ave. Chicago, IL 60642, (773) 227-3114. In the matters of the personal property for the individuals listed below:
Unit - Customer 0114 - Walsh, Daryl 0126 - Hiegel, ADRIENNE 0130 - Pennington, Paris 0410 - Lathan, Kris 0416 - Hibbler, Andrea 0539 - Turner, Franklin 0567 - Reed, Lawrence 0720 - Fowler, Anais 0763 - Collozo, Emilio
Public Storage/ PS Orangoeco, Inc. Purchases must be made with cash only and paid at the time of sale.
All goods are sold as is and must be removed at the time of purchase. Sale is subjected to adjournment.
7/10, 7/17/17 #1032067
NOTICE OF PUBLIC SALE OF PERSONAL PROPERTY
Notice is hereby given that pursuant to Section 4 of the Self-Service Storage Facility Act, State of Illinois, that Public Storage / PS Orangoeco, Inc. will conduct sale(s) at Public Storage by Competitive bidding on July 26th, 2017 at 10:00 am on the Premises where property has been stored, which are located at Public Storage #08066, 947 West Van Buren, Chicago, IL 60607, (312) 421-0049. In the matters of the personal property for the individuals listed below:
Unit - Customer 0114 - Walsh, Daryl 0126 - Hiegel, ADRIENNE 0130 - Pennington, Paris 0410 - Lathan, Kris 0416 - Hibbler, Andrea 0539 - Turner, Franklin 0567 - Reed, Lawrence 0720 - Fowler, Anais 0763 - Collozo, Emilio
Public Storage/ PS Orangoeco, Inc. Purchases must be made with cash only and paid at the time of sale.
All goods are sold as is and must be removed at the time of purchase. Sale is subjected to adjournment.
7/10, 7/17/17 #1032067
NOTICE OF PUBLIC SALE OF PERSONAL PROPERTY
Notice is hereby given that pursuant to Section 4 of the Self-Service Storage Facility Act, State of Illinois, that Public Storage / PS Orangoeco, Inc. will conduct sale(s) at Public Storage by Competitive bidding on July 25th, 2017 at 11:00 am on the Premises where property has been stored, which are located at Public Storage #22333/ 0211 - Kriovkuca, Dusan 0254 - Schmidt, Frank 0345 - Leavitt, Irwin 0403 - Escamilla, Ine 0575 - Penarietti, Carmen Rosa 0603 - Schmidt, Peggy 0548 - Furkal, Oleksandr
Public Storage / PS Orangoeco, Inc. Purchases must be made with cash/credit card only and paid at the time of sale.
All goods are sold as is and must be removed at the time of purchase. Sale is subjected to adjournment.
7/10, 7/17/17 #1032034

Storage - Legal
NOTICE OF PUBLIC SALE OF PERSONAL PROPERTY
Notice is hereby given that pursuant to Section 4 of the Self-Service Storage Facility Act, State of Illinois, that Public Storage / PS Orangoeco, Inc. will conduct sale(s) at Public Storage by Competitive bidding on July 25th, 2017 at 10:45 am on the Premises where property has been stored, which are located at Public Storage #20621/ 1385 East Dundee Road, Palatine IL 60074, 847-359-3280. In the matters of the personal property for the individuals listed below:
A012 - Porwiz, Jan F017 - Daka, Gordianus F024 - Trujillo, Ignacio F062 - Lopez, Juan M. G050 - Smith, Gabriel H015 - Ripplinger, Matthew A. H023 - Gyuerkoets, Steven J059 - Garcia, Alex J155 - Lopez, Raquel K068 - Menis, Lois K108 - Lucena, Anthony
Public Storage / PS Orangoeco, Inc. Purchases must be made with cash/credit card only and paid at the time of sale.
All goods are sold as is and must be removed at the time of purchase. Sale is subjected to adjournment.
7/10, 7/17/17 #1032026
NOTICE OF PUBLIC SALE OF PERSONAL PROPERTY
Notice is hereby given that pursuant to Section 4 of the Self-Service Storage Facility Act, State of Illinois, that Public Storage / PS Orangoeco, Inc. will conduct sale(s) at Public Storage by Competitive bidding on July 25th, 2017 at 11:30 am on the Premises where property has been stored, which are located at Public Storage #08068 / 3320 West Lake Avenue, Glenview, IL 60026-1211, 847-486-0536. In the matters of the personal property for the individuals listed below:
0211 - Kriovkuca, Dusan 0254 - Schmidt, Frank 0345 - Leavitt, Irwin 0403 - Escamilla, Ine 0575 - Penarietti, Carmen Rosa 0603 - Schmidt, Peggy 0548 - Furkal, Oleksandr
Public Storage / PS Orangoeco, Inc. Purchases must be made with cash/credit card only and paid at the time of sale.
All goods are sold as is and must be removed at the time of purchase. Sale is subjected to adjournment.
7/10, 7/17/17 #1032034

Storage - Legal
Notice of Lien Sale / Public Auction
In accordance with the provisions of State law, there being due and unpaid charges for which the undersigned is entitled to satisfy an owner and/or manager's lien of the goods hereinafter described and stored at the Life Storage location(s) listed below. And, due notice having been given, to the owner of said property and all parties known to claim an interest therein, and the time specified in such notice for payment of such having expired, the goods will be sold at public auction at the below stated location(s) to the highest bidder or otherwise disposed of on Friday, July 21st, 2017 at 09:00 am in the order below. #555, 6505 Oakton St Morton Grove IL 60053 Ph 847.966.1899: 1503 ASC Shantel, 584 ASC Shantel, 6001 N Plum Grove Rd Schaumburg IL 60173 Ph 847.843.1027: 710 Jacqueline D Hall. #568, 1950 N Washington St Naperville IL 60563 Ph 630.505.0001: 1089 Robert Elkins, 2012 Pradeep Pal. #422, 232 South Lake Street Aurora IL 60506 Ph 630.966.0965: 1029 Anthony A Taylor, 2135 Raymond D George, 3020 Detrick L Walton, 3025 Martel Pryor. #554, 1650 N Randall Rd Aurora IL 60506 Ph 630.907.9870: 1018 Casha Wright, 1118 Sherry Renee Sartin, 1184 Donald Jones, 1267 Juan Gomez, 1283 Stanley White, 1310 Larry Flowers, 2017 Marcus Shipp, 2047 Nicole Johnson, 2073 Steven T Kolen, 3101 Penny Jackson, 3211 Steven T Koleno, 3219 Peggy Keating, 3219 Peggy Lynn Keating. #442, 2625 E Main Street Saint Charles IL 60174 Ph 630.584.0580: 4111 Jamie L Souza. #560, 450 Airport Rd Elgin IL 60123 Ph 847.886.1834: 293 Julia Ann Mladky, 412 Vishi Songkharn, 487 Travis Steven T Kolen, 550 Morales, 551 Jillivona McJefferson. #549, 2253 Randall Rd Carpentersville IL 60110 Ph 847.836.2431: 231 Ryan Crowthers. #548, 2301 W Algonquin Rd Algonquin IL 60102 Ph 847.658.5968: 1188 Dustin Mysynski, 255 James Foster. #567, 1455 S Barton Rd Bartonville IL 60010 Ph 630.543.3100: 1174 Alana Michaels, 1192 Alana Michaels, 1188 Susan Vermaat, 1270 Aleksandra Rebic. Pub: 7/3 & 7/10/2017 1030837

Storage - Legal
Notice of Lien Sale / Public Auction
In accordance with the provisions of State law, there being due and unpaid charges for which the undersigned is entitled to satisfy an owner and/or manager's lien of the goods hereinafter described and stored at the Life Storage location(s) listed below. And, due notice having been given, to the owner of said property and all parties known to claim an interest therein, and the time specified in such notice for payment of such having expired, the goods will be sold at public auction at the below stated location(s) to the highest bidder or otherwise disposed of on Thursday, July 20th, 2017 at 09:00 am in the order below. #563, 3200 Holeman Ave South Chicago Heights IL 60411 Ph 708.754.6820: E54 Kenneth Garcia. #562, 1930 W Cicero Ave Matteson IL 60443 Ph 708.748.4967: 1923 Dwayne Ricks, 196 Ayita Dwyane, 236 Dennis Tibbs, 322 Sheree Reed, 336 Sherry Davis, 389 Kaylan Howard, 582 Tari Shredley. #566, 8531 W 191st Ave Oak Park IL 60438 Ph 708.464.3015: 0210 Toni Cogan, 1093 Leandra Irvin, 2015 Tangina Johnson, 500 Robin Hicks. #417, 11255 184th Place Ontario Park IL 60467 Ph 708.478.0561: 514 Richard E. Perez Jr., 1036 Adeline C. Sestak. #480, 5253 West 111th Street Alsip IL 60803 Ph 708.425.4499: #42 Don Montgomery, F104 Raymond Kaminski, K629 Mark Zotto. #556, 7720 W 79th St Bridgeview IL 60455 Ph 708.924.9484: 1721 Colleen C Quinlivan, 1826 Adam Lattaris, 1844 Melissa L Love. #570, 405 Shawmut Ave La Grange Park IL 60525 Ph 708.352.0001: 1211 Dennis Ryan. #569, 1800 Des Plaines Ave Forest Park IL 60130 Ph 708.209.1100: 1189 Frank Hoekendorf, 1216 Theresa Robbins, 2258 Maurice Durr, 2289 Peggy Hill, 2307 Beth Edmonds, 2397 Floriella Bailon. #559, 953 S State Route 83 Elmhurst IL 60126 Ph 630.679.2522: 00013 Salvatore Miglio, 00013 Sal Miglio, A1112 Rolando Gonzalez, B1175 Kate Barberi, C1170 Wade Neudel, C1163 Malchus Ervin. #557, 426 S Westgate St Addison IL 60101 Ph 630.543.6303: 1009 Sam Ristic. Pub: 7/3 & 7/10/2017 1030839

**Englewood Elevated Railroad Connector at 59th Street (from Hoyne to Lowe)
Summary of Public Meeting**

IMPACTED PROPERTY OWNER LETTERS



CHICAGO DEPARTMENT OF TRANSPORTATION
CITY OF CHICAGO

Improvement of the 59th Street Line
Between Hoyne Avenue and Lowe Avenue
CDOT No.: E - 5 - 4 8 2

RE: Property Tax Number 20-18-224-017 (5829 S. Damen Avenue)

July 7, 2017

Mr. A Haroon
3314 W. Albion Ave.
Lincolnwood, IL 60712

Dear Sir:

The purpose of this letter is to notify you that the Chicago Department of Transportation (CDOT) and the Chicago Department of Planning and Development (CDPD) are presently in the preliminary engineering phase of a study for the improvement of Norfolk Southern's (NS) Englewood Line. The proposed improvement generally consists of converting the former elevated rail line to a multi-use trail.

It is the policy of the Department of Transportation to provide all interested persons an opportunity to become acquainted with potential proposal and to allow them the opportunity to express their views at those stages of a proposed project when the flexibility to respond to those views still exists. When a proposed project would have impacts on the surrounding areas, or where right-of-way would be acquired by the City, a public meeting will be held.

Based upon a review of the 2016 tax records of Cook County, you are indicated to be the owner of the property located at 5829 South Damen Avenue which is south of the NS Englewood Line as shown on the enclosed aerial photograph. The potential right-of-way that may be acquired from your property for this improvement is indicated on the enclosed aerial preliminary plan exhibit. This potential right-of-way acquisition would be used to create an access to the elevated trail.

At the end of this letter is an area where you can indicate your comments, if any. Please indicate the appropriate response and return one copy to us in the enclosed, self-addressed stamped envelope. We would appreciate receiving any comments you may have by July 28, 2017. You may also attend the open house public meeting scheduled for July 13, 2017 from 5:30 PM to 7:00 PM at the Charles R. Henderson Elementary School.

Upon completion of our study, a report will be prepared describing the proposed work and submitted to IDOT and FHWA for approval. After approval is received, we will proceed with the

contact you regarding any necessary acquisition.

Please note that your response, or lack thereof, will in no way influence the amount of compensation you will receive for your property if acquired as part of the project.

If, after reviewing this letter, you have any questions or wish to discuss this improvement in more detail, please contact Nelson Chueng of the CDPD at 312/744-5756.

Very truly yours,

A handwritten signature in black ink, appearing to read "Daniel Burke". The signature is written in a cursive style with a large, prominent initial "D".

Daniel Burke, P.E., S.E.
Deputy Commissioner
Division of Engineering

Enclosure

Check the appropriate response:

I have no comments at this time.

I have noted my comments on the bottom of this page.

I would like to discuss this further in a telephone conversation. I will call you.

Signature of Owner _____

Date _____



CHICAGO DEPARTMENT OF TRANSPORTATION
CITY OF CHICAGO

Improvement of the 59th Street Line
Between Hoyne Avenue and Lowe Avenue
CDOT No.: E - 5 - 4 8 2

RE: Property Tax Number 20-18-224-018 (5831 S. Damen Avenue)
Property Tax Number 20-18-224-020 (5837 S. Damen Avenue)

July 7, 2017

Mr. Syed Razi
6332 N. Mozart Street
Chicago, IL 60659

Dear Sir:

The purpose of this letter is to notify you that the Chicago Department of Transportation (CDOT) and the Chicago Department of Planning and Development (CDPD) are presently in the preliminary engineering phase of a study for the improvement of Norfolk Southern's (NS) Englewood Line. The proposed improvement generally consists of converting the former elevated rail line to a multi-use trail.

It is the policy of the Department of Transportation to provide all interested persons an opportunity to become acquainted with potential proposal and to allow them the opportunity to express their views at those stages of a proposed project when the flexibility to respond to those views still exists. When a proposed project would have impacts on the surrounding areas, or where right-of-way would be acquired by the City, a public meeting will be held.

Based upon a review of the 2016 tax records of Cook County, you are indicated to be the owner of the properties located at 5831 and 5837 South Damen Avenue which is south of the NS Englewood Line as shown on the enclosed aerial photograph. The potential right-of-way that may be acquired from your property for this improvement is indicated on the enclosed aerial preliminary plan exhibit. This potential right-of-way acquisition would be used to create an access to the elevated trail.

At the end of this letter is an area where you can indicate your comments, if any. Please indicate the appropriate response and return one copy to us in the enclosed, self-addressed stamped envelope. We would appreciate receiving any comments you may have by July 28, 2017. You may also attend the open house public meeting scheduled for July 13, 2017 from 5:30 PM to 7:00 PM at the Charles R. Henderson Elementary School.


Upon completion of our study, a report will be prepared describing the proposed work and submitted to IDOT and FHWA for approval. After approval is received, we will proceed with the

contact you regarding any necessary acquisition.

Please note that your response, or lack thereof, will in no way influence the amount of compensation you will receive for your property if acquired as part of the project.

If, after reviewing this letter, you have any questions or wish to discuss this improvement in more detail, please contact Nelson Chueng of the CDPD at 312/744-5756.

Very truly yours,

A handwritten signature in black ink that reads "Daniel Burke". The signature is written in a cursive, flowing style.

Daniel Burke, P.E., S.E.
Deputy Commissioner
Division of Engineering

Enclosure

Check the appropriate response:

I have no comments at this time.

I have noted my comments on the bottom of this page.

I would like to discuss this further in a telephone conversation. I will call you.

Signature of Owner _____

Date _____



**NORFOLK SOUTHERN'S
ENGLEWOOD LINE**

20-18-224-018

20-18-224-020

DAMEN AVENUE



Improvement of the 59th Street Line



CHICAGO DEPARTMENT OF TRANSPORTATION
CITY OF CHICAGO

Improvement of the 59th Street Line
Between Hoyne Avenue and Lowe Avenue
CDOT No.: E - 5 - 4 8 2

RE: Property Tax Number 20-18-224-019 (5833 S. Damen Avenue)

July 7, 2017

Mr. Lee Roy Ball
19401 Lake Lynwood Drive
Lynwood, IL 60411

Dear Sir:

The purpose of this letter is to notify you that the Chicago Department of Transportation (CDOT) and the Chicago Department of Planning and Development (CDPD) are presently in the preliminary engineering phase of a study for the improvement of Norfolk Southern's (NS) Englewood Line. The proposed improvement generally consists of converting the former elevated rail line to a multi-use trail.

It is the policy of the Department of Transportation to provide all interested persons an opportunity to become acquainted with potential proposal and to allow them the opportunity to express their views at those stages of a proposed project when the flexibility to respond to those views still exists. When a proposed project would have impacts on the surrounding areas, or where right-of-way would be acquired by the City, a public meeting will be held.

Based upon a review of the 2016 tax records of Cook County, you are indicated to be the owner of the property located at 5833 South Damen Avenue which is south of the NS Englewood Line as shown on the enclosed aerial photograph. The potential right-of-way that may be acquired from your property for this improvement is indicated on the enclosed aerial preliminary plan exhibit. This potential right-of-way acquisition would be used to create an access to the elevated trail.

At the end of this letter is an area where you can indicate your comments, if any. Please indicate the appropriate response and return one copy to us in the enclosed, self-addressed stamped envelope. We would appreciate receiving any comments you may have by July 28, 2017. You may also attend the open house public meeting scheduled for July 13, 2017 from 5:30 PM to 7:00 PM at the Charles R. Henderson Elementary School.

Upon completion of our study, a report will be prepared describing the proposed work and submitted to IDOT and FHWA for approval. After approval is received, we will proceed with the plan preparation and land acquisition phase. In that phase, a representative of the City will

contact you regarding any necessary acquisition.

Please note that your response, or lack thereof, will in no way influence the amount of compensation you will receive for your property if acquired as part of the project.

If, after reviewing this letter, you have any questions or wish to discuss this improvement in more detail, please contact Nelson Chueng of the CDPD at 312/744-5756.

Very truly yours,

A handwritten signature in black ink that reads "Daniel Burke". The signature is written in a cursive, flowing style.

Daniel Burke, P.E., S.E.
Deputy Commissioner
Division of Engineering

Enclosure

Check the appropriate response:

I have no comments at this time.

I have noted my comments on the bottom of this page.

I would like to discuss this further in a telephone conversation. I will call you.

Signature of Owner _____

Date _____



**NORFOLK SOUTHERN'S
ENGLEWOOD LINE**

20-18-224-019

DAMEN AVENUE



Improvement of the 59th Street Line



CHICAGO DEPARTMENT OF TRANSPORTATION

CITY OF CHICAGO

Improvement of the 59th Street Line
Between Hoyne Avenue and Lowe Avenue
CDOT No.: E - 5 - 4 8 2

RE: Property Tax Number 20-17-124-034 Property Tax Number 20-17-124-037
 Property Tax Number 20-17-124-035 Property Tax Number 20-17-124-027
 Property Tax Number 20-17-124-008 Property Tax Number 20-17-124-028
 Property Tax Number 20-17-124-009 Property Tax Number 20-17-124-029
 Property Tax Number 20-17-124-010

July 7, 2017

Mr. John Hanches
5515 N. Forest Glen
Chicago, IL 60630

Dear Sir:

The purpose of this letter is to notify you that the Chicago Department of Transportation (CDOT) and the Chicago Department of Planning and Development (CDPD) are presently in the preliminary engineering phase of a study for the improvement of Norfolk Southern's (NS) Englewood Line. The proposed improvement generally consists of converting the former elevated rail line to a multi-use trail.

It is the policy of the Department of Transportation to provide all interested persons an opportunity to become acquainted with potential proposal and to allow them the opportunity to express their views at those stages of a proposed project when the flexibility to respond to those views still exists. When a proposed project would have impacts on the surrounding areas, or where right-of-way would be acquired by the City, a public meeting will be held.

Based upon a review of the 2016 tax records of Cook County, you are indicated to be the owner of the property located on the east side of South Ashland Avenue immediately south of the NS Englewood Line as shown on the enclosed aerial photograph. The potential right-of-way that may be acquired from your property for this improvement is indicated on the enclosed aerial preliminary plan exhibit. This potential right-of-way acquisition would be used to create an access to the elevated trail.

At the end of this letter is an area where you can indicate your comments, if any. Please indicate the appropriate response and return one copy to us in the enclosed, self-addressed stamped envelope. We would appreciate receiving any comments you may have by July 28, 2017. You may also attend the open house public meeting scheduled for July 13, 2017 from 5:30 PM to 7:00 PM at the Charles R. Henderson Elementary School.

Upon completion of our study, a report will be prepared describing the proposed work and submitted to IDOT and FHWA for approval. After approval is received, we will proceed with the plan preparation and land acquisition phase. In that phase, a representative of the City will contact you regarding any necessary acquisition.

Please note that your response, or lack thereof, will in no way influence the amount of compensation you will receive for your property if acquired as part of the project.

If, after reviewing this letter, you have any questions or wish to discuss this improvement in more detail, please contact Nelson Chueng of the CDPD at 312/744-5756.

Very truly yours,

A handwritten signature in black ink that reads "Daniel Burke". The signature is written in a cursive, flowing style.

Daniel Burke, P.E., S.E.
Deputy Commissioner
Division of Engineering

Enclosure

Check the appropriate response:

- I have no comments at this time.
- I have noted my comments on the bottom of this page.
- I would like to discuss this further in a telephone conversation. I will call you.

Signature of Owner _____ Date _____

**NORFOLK SOUTHERN'S
ENGLEWOOD LINE**

ASHLAND AVENUE

20-17-124-034

20-17-124-037

20-17-124-035

20-17-124-008

20-17-124-027

20-17-124-009

20-17-124-028

20-17-124-010

20-17-124-029

Improvement of the 59th Street Line





CHICAGO DEPARTMENT OF TRANSPORTATION
CITY OF CHICAGO

Improvement of the 59th Street Line
Between Hoyne Avenue and Lowe Avenue
CDOT No.: E - 5 - 4 8 2

RE: Property Tax Number 20-18-124-001 (5800-5820 S. Seeley Avenue)

July 7, 2017

Rev. Vesta L. Dixon
Evening Star Missionary Baptist Church
2050 W. 59th Street
Chicago, IL 60636

Dear Rev. Dixon:

The purpose of this letter is to notify you that the Chicago Department of Transportation (CDOT) and the Chicago Department of Planning and Development (CDPD) are presently in the preliminary engineering phase of a study for the improvement of Norfolk Southern's (NS) Englewood Line. The proposed improvement generally consists of converting the former elevated rail line to a multi-use trail.

It is the policy of the Department of Transportation to provide all interested persons an opportunity to become acquainted with potential proposal and to allow them the opportunity to express their views at those stages of a proposed project when the flexibility to respond to those views still exists. When a proposed project would have impacts on the surrounding areas, or where right-of-way would be acquired by the City, a public meeting will be held.

Based upon a review of the 2016 tax records of Cook County, you are indicated to be the owner of the properties located at 5800-5820 South Seeley Avenue which is north of the NS Englewood Line as shown on the enclosed aerial photograph. The potential right-of-way that may be acquired from your property for this improvement is indicated on the enclosed aerial preliminary plan exhibit. This potential right-of-way acquisition would be used to create an access to the elevated trail.

At the end of this letter is an area where you can indicate your comments, if any. Please indicate the appropriate response and return one copy to us in the enclosed, self-addressed stamped envelope. We would appreciate receiving any comments you may have by July 28, 2017. You may also attend the open house public meeting scheduled for July 13, 2017 from 5:30 PM to 7:00 PM at the Charles R. Henderson Elementary School.

Upon completion of our study, a report will be prepared describing the proposed work and submitted to IDOT and FHWA for approval. After approval is received, we will proceed with the

plan preparation and land acquisition phase. In that phase, a representative of the City will contact you regarding any necessary acquisition.

Please note that your response, or lack thereof, will in no way influence the amount of compensation you will receive for your property if acquired as part of the project.

If, after reviewing this letter, you have any questions or wish to discuss this improvement in more detail, please contact Nelson Chueng of the CDPD at 312/744-5756.

Very truly yours,

A handwritten signature in black ink, appearing to read "Daniel Burke". The signature is written in a cursive, flowing style.

Daniel Burke, P.E., S.E.
Deputy Commissioner
Division of Engineering

Enclosure

Check the appropriate response:

I have no comments at this time.

I have noted my comments on the bottom of this page.

I would like to discuss this further in a telephone conversation. I will call you.

Signature of Owner _____

Date _____

HOYNE AVENUE

20-18-124-001

**NORFOLK SOUTHERN'S
ENGLEWOOD LINE**

Improvement of the 59th Street Line



**Englewood Elevated Railroad Connector at 59th Street (from Hoyne to Lowe)
Summary of Public Meeting**

SIGN-IN SHEETS



Project: Improvement of the 59th Street Line Trail Between Hoyne Avenue and Lowe Avenue

Charles R Henderson Elementary School - July 13, 2017, 5:30-7:00 PM

Please print your name and address below:

	Name	Address / Email	Representing
P L E A S E E P R I N T	1. JASON HUNT	6013 S PIERCE ST JASUN.VA@GMAIL.COM	Self <input checked="" type="checkbox"/> Company or Organization: <input type="checkbox"/>
	2. BROWN, GEORGE	5641 S WILCOFF AVE CHICAGO IL 60636	Self <input checked="" type="checkbox"/> Company or Organization: <input type="checkbox"/>
	3. Patrick Bullard	5616 So. Wood St CHICAGO IL 60636	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	4. Calvin Taylor	5630 S. Wood Chicago ill 60636	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	5. Pasqua Anthony G. II	1622 W 6th ST CHICAGO, IL 60609	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	6. Edoardo	4536 S. Polary 60609	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	7. BWEZA Itadaji	5011 N Artesian Ave #3 CHICAGO, IL 60659	Self <input checked="" type="checkbox"/> Company or Organization: <input type="checkbox"/>
	8. Gregory Grant	611 W. Jefferson St suite 201 GTGrant@LaneConstruct.com	Self <input type="checkbox"/> Company or Organization: <input checked="" type="checkbox"/>
	9. MARK THOMAS	GARFIELD PRODUCE COMPANY MARK@GARFIELDPRODUCE.COM	Self <input type="checkbox"/> Company or Organization: <input checked="" type="checkbox"/>
	10. Giovanni Valdez	G10M21@gmail.com	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	11. Michael Andersen	mjandersen@laneconstruction.com	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	12. Eduardo Bey	cmibabywrappers@gmail.com	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	13. Kumba Yero Sample	11126 S. Sangamon Chicago IL 60643	Self <input type="checkbox"/> Company or Organization: <input checked="" type="checkbox"/> AKA Solutions
	14. ALDTONI FRULKE	16th WARD	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	15. Deborah Smith	United Englewood UNE_CDP@yahoo.com	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>

Project: Improvement of the 59th Street Line Trail Between Hoyne Avenue and Lowe Avenue

Charles R Henderson Elementary School - July 13, 2017, 5:30-7:00 PM

Please print your name and address below:

	Name	Address / Email	Representing
P	1. <i>Janita Jones</i>	<i>5821 S. Hurstfield</i>	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
L	2. <i>Melvin Thomas</i>	<i>5654 S Winchester</i>	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
E	3. <i>Jack Blue</i>	<i>Box 2918 Chicago, IL 60690</i>	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
A	4. <i>Frances Smith</i>	<i>2050 W. 59th St Chicago 60628</i>	Self <input type="checkbox"/> Company or Organization: <input checked="" type="checkbox"/>
S	5. <i>Joseph Massie</i>	<i>6008 S Wolcott</i>	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
E	6. <i>Charlene Massie</i>	<i>5630 S. WOOD</i>	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
E	7. <i>Renee Laurene Harris</i>	<i>5652 S. WOOD CHICAGO 60638</i>	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
P	8. <i>Harry Rhodes</i>	<i>5814 S. Wood St. Chicago 60636</i>	Self <input type="checkbox"/> Company or Organization: <input checked="" type="checkbox"/> <i>Growing Home</i>
R	9. <i>Mark Yelverton</i>	<i>5531 S. Honore</i>	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
I	10. <i>Maitha Raddie</i>	<i>8231 S Throop</i>	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
N	11. <i>Marjorie Jackson Ivy</i>	<i>5650 S. Wolcott</i>	Self <input type="checkbox"/> Company or Organization: <input checked="" type="checkbox"/> <i>CPS</i>
T	12. <i>Gloria Williams</i>	<i>5719 SO. Winchester voice of west Englewood@gmail.com</i>	Self <input type="checkbox"/> Company or Organization: <input checked="" type="checkbox"/>
	13. <i>John M. Barcevas</i>	<i>6018 S. Montague john-barcevas@rush.edu</i>	Self <input type="checkbox"/> Company or Organization: <input checked="" type="checkbox"/> <i>Rush Univ</i>
	14. <i>Rose Robinson</i>		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	15. <i>Darius Jones</i>	<i>1545 S State</i>	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>



Project: Improvement of the 59th Street Line Trail Between Hoyne Avenue and Lowe Avenue
Charles R Henderson Elementary School - July 13, 2017, 5:30-7:00 PM

Please print your name and address below:

	Name	Address / Email	Representing
P L E A S E P R I N T	1. Darlene Jones	5717 S. WOLCOTT Darlene 4709@att.net	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	2. Crystal [unclear]	4406 S. WYBE PL Chicago, IL	Self <input checked="" type="checkbox"/> Company or Organization: <input type="checkbox"/>
	3. Jermon Montgomery	Jermon Montgomery jermon.montgomery@cityofchicago.org	Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	4.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	5.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	6.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	7.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	8.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	9.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	10.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	11.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	12.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	13.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	14.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>
	15.		Self <input type="checkbox"/> Company or Organization: <input type="checkbox"/>

**Englewood Elevated Railroad Connector at 59th Street (from Hoyne to Lowe)
Summary of Public Meeting**

COMMENTS RECEIVED



CHICAGO DEPARTMENT
OF TRANSPORTATION



CHICAGO DEPARTMENT OF
PLANNING & DEVELOPMENT

WE WANT YOUR INPUT!

(Contact information is optional)

Name: CARVIN TAYLOR

Address: 5622 S. WOOD

City: CHICAGO State: IL ZIP: 60636

Phone: 1-630-254-1117 Email: _____

I like the conspot
Look good got my vote



CHICAGO DEPARTMENT
OF TRANSPORTATION



CHICAGO DEPARTMENT OF
PLANNING & DEVELOPMENT

WE WANT YOUR INPUT!

(Contact information is optional)

Name: J. BWE

Address: Boy ~~St~~ 2918

City: Chicago State: Ill. ZIP: 60690

Phone: 773-954-6805 Email:

Worth the money improvements to the
community.



WE WANT YOUR INPUT!

(Contact Information Is optional)

Name: Bineza Itaagi

Address: _____

City: Chicago State: IL ZIP: _____

Phone: _____ Email: nitaggi@msn.com

How will you continue to encourage community input?

Will there be ongoing evaluations to gauge the positive or negative impacts of the trail on the surrounding communities?

Will residents have priority for all employment opportunities the trail will provide? How can we ensure that the trail doesn't create an opportunity for gentrification/outside business infiltration into our neighborhoods?



CHICAGO DEPARTMENT
OF TRANSPORTATION



CHICAGO DEPARTMENT OF
PLANNING & DEVELOPMENT

WE WANT YOUR INPUT!

(Contact information is optional)

Name: Deborah Smith, United Englewood CDC

Address: 70 BOX 2010 26

City: Chgo

State: IL

ZIP: 60620

Phone: 708-980

Email: 6133

I'm concern that we do not create an atmosphere
in which longtime resident face gentrification
pressure as a result of the bike trail/recreational path.
All activities must be inclusive of the wants and needs
of All of Englewood Existing Residents



WE WANT YOUR INPUT!

(Contact information is optional)

Name: Charlene Strickland

Address: 5630 S. Wood

City: Chgo State: IL ZIP: 60636

Phone: 773-436-6687 Email: CharleneStrickland123@gmail.com



CHICAGO DEPARTMENT
OF TRANSPORTATION



CHICAGO DEPARTMENT OF
PLANNING & DEVELOPMENT

WE WANT YOUR INPUT!

(Contact information is optional)

Name: Patrick Bullard

Address: 5616 So Wood ST

City: Chicago

State: IL ZIP: 60636

Phone: 773 552-4723

Email: patrick.bullard62pb@gmail.com



WE WANT YOUR INPUT!

(Contact Information is optional)

Name: Rose Robinson

Address: 5615 S. Wood

City: Chicago

State: IL ZIP: 60636

Phone: 312-414-9085 Email: WOWE



WE WANT YOUR INPUT!

(Contact information is optional)

Name: MR. YELVERTON

Address: _____

City: _____ State: _____ ZIP: _____

Phone: _____ Email: MRkyelverton@yahoo.com

CONCERNS: AFTER Dusk lighting, CAMERAS For security,
Emergency call boxes (such as University of Chicago),
SPECIAL ATTN PER EACH CPD beat that TRAIL CROSSES!



CHICAGO DEPARTMENT
OF TRANSPORTATION



CHICAGO DEPARTMENT OF
PLANNING & DEVELOPMENT

WE WANT YOUR INPUT!

(Contact information is optional)

Name: EdUARdo E. Bey ©

Address: 5337 S. Bishop

City: Chicago

State: IL ZIP: 60609

Phone: (708) 305 7249

Email: chibablywrapoure

I'm leaning in the direction of more economical/
stance for the environment and all the neighbors
in the community to see the growth



CHICAGO DEPARTMENT
OF TRANSPORTATION



CHICAGO DEPARTMENT OF
PLANNING & DEVELOPMENT

WE WANT YOUR INPUT!

(Contact information is optional)

Name: Pastor Anthony Gill c/o Philadelphia Church of God In Christ

Address: 1622 W. 61st St.

City: Chicago State: IL ZIP: 60636

Phone: 773-737-8822 Email: philadelphia@pcogic.comcastbiz.net

"Safety is of utmost importance"

"Restaurants" - Family - NOT FAST FOOD.

"Eliminate" - stand alone & other liquor stores.



CHICAGO DEPARTMENT
OF TRANSPORTATION



CHICAGO DEPARTMENT OF
PLANNING & DEVELOPMENT

WE WANT YOUR INPUT!

(Contact information is optional)

Name: Evelyn Rawo

Address: _____

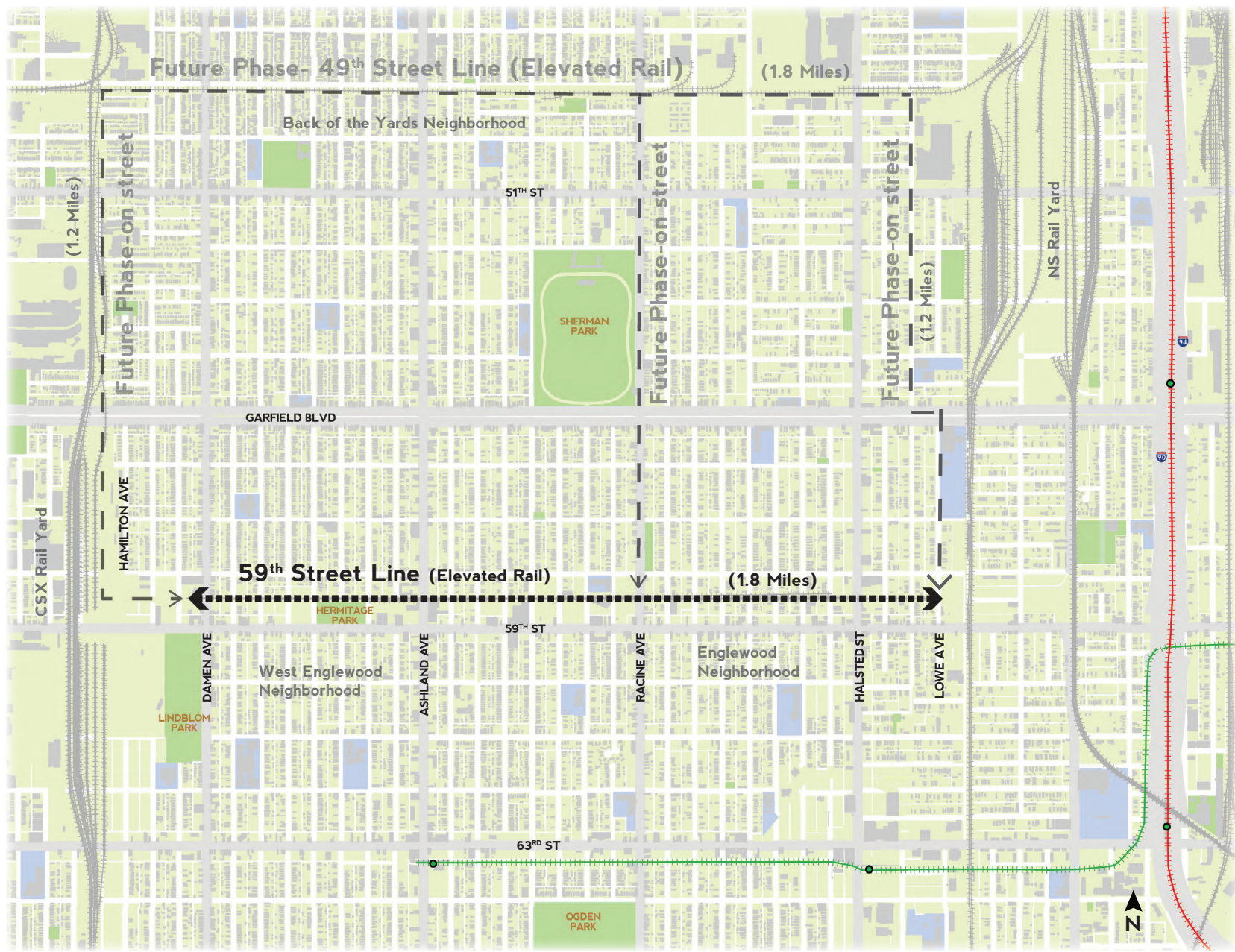
City: _____ State: _____ ZIP: _____

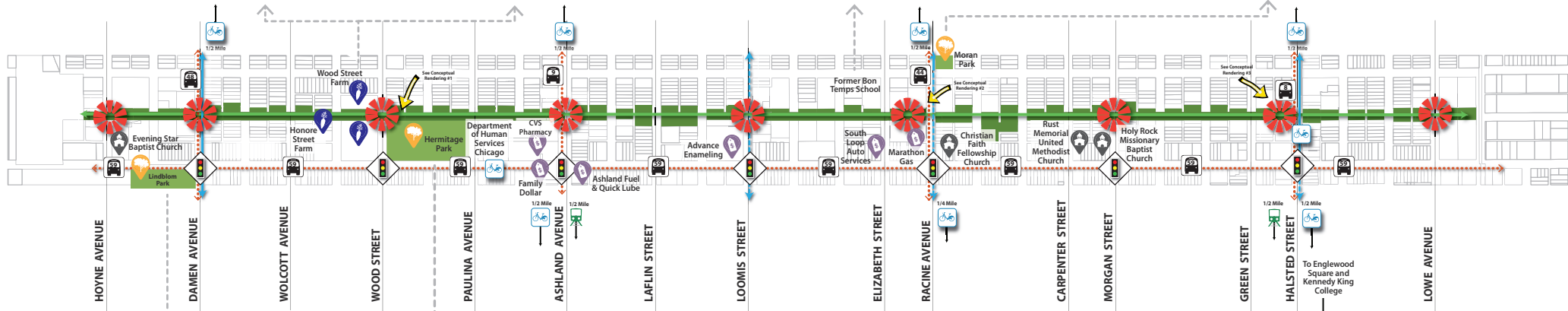
Phone: _____ Email: _____

Kids Splash Pad

**Englewood Elevated Railroad Connector at 59th Street (from Hoyne to Lowe)
Summary of Public Meeting**

EXHIBITS

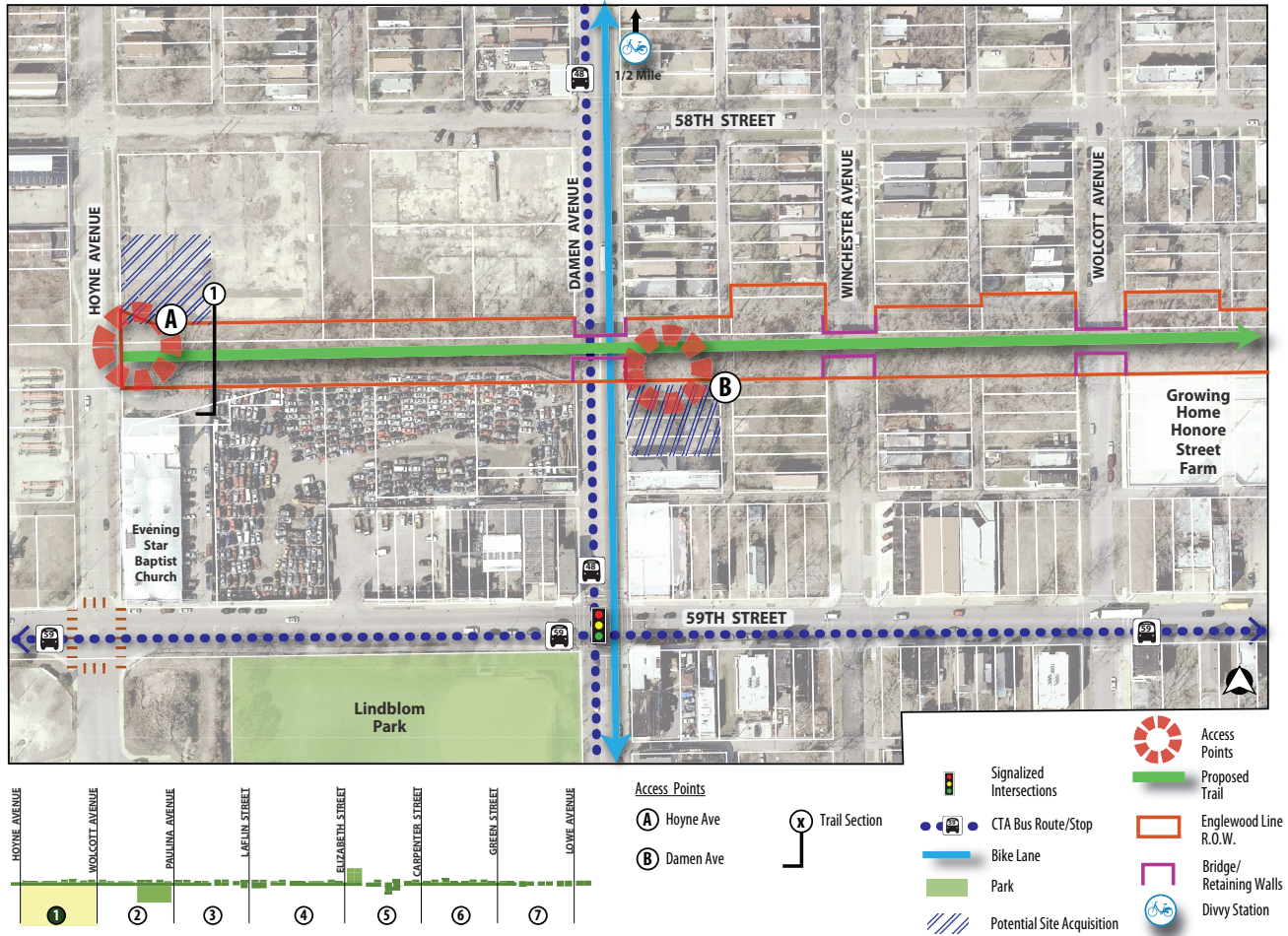




- Signalized Intersections
- Bike Lane
- Park
- CTA Bus Route/Stop
- Proposed Access Points
- Proposed Trail
- Divvy Station



1: HOYNE AVE. to WOLCOTT AVE.



CONCEPT SECTION 1

1. Between Hoyne Avenue and Damen Avenue



2: WOLCOTT AVE. TO PAULINA AVE.



CONCEPT SECTION 2

②. Between Wood Street and Hermitage Avenue

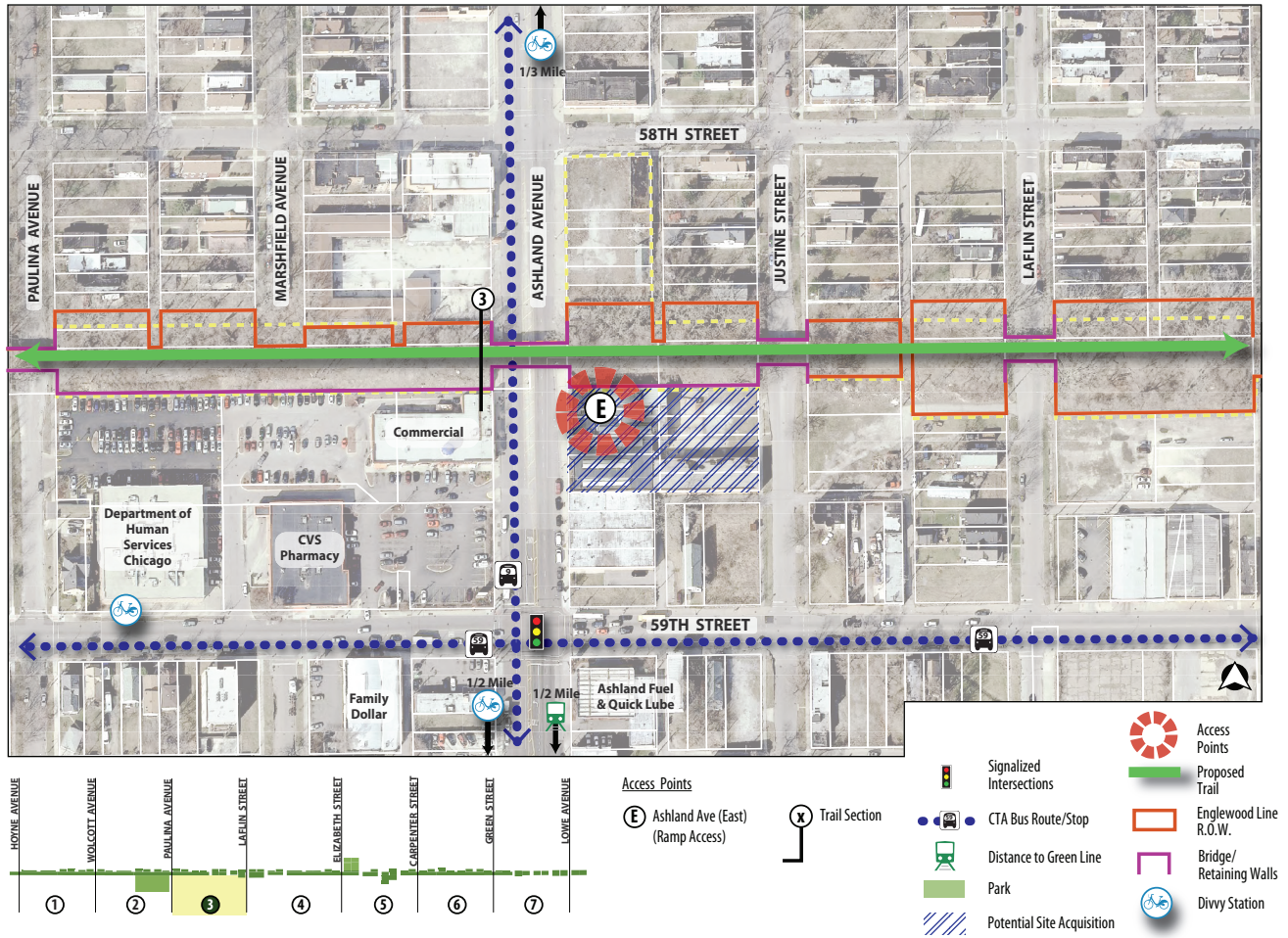


CONCEPTUAL RENDERING # 1

Access Point at Hermitage Park between Paulina Avenue and Wood Street



3: PAULINA AVE. TO LAFLIN ST.

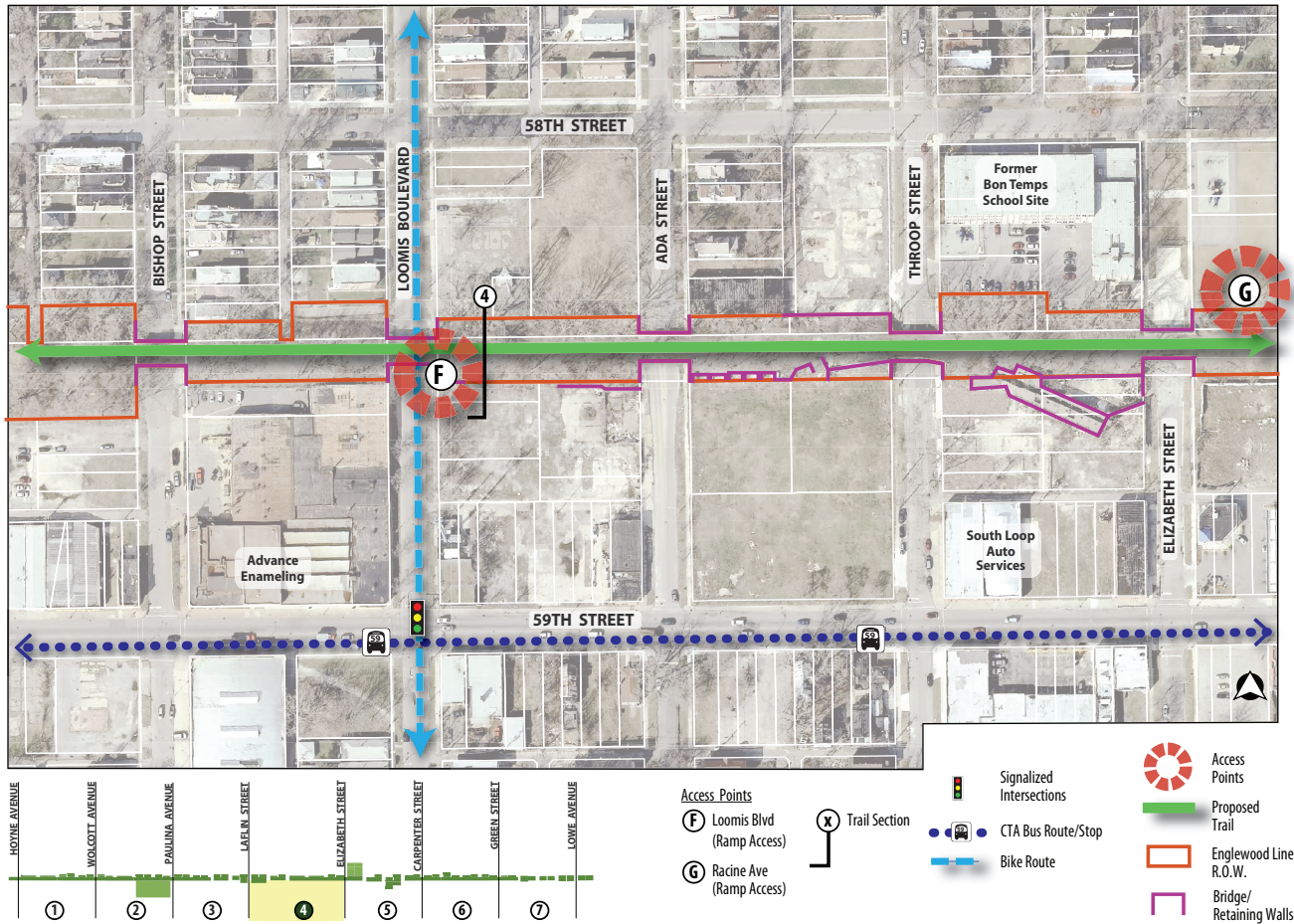


CONCEPT SECTION 3

3. Between Marshfield Avenue and Ashland Avenue



4: LAFLIN ST. TO ELIZABETH ST.

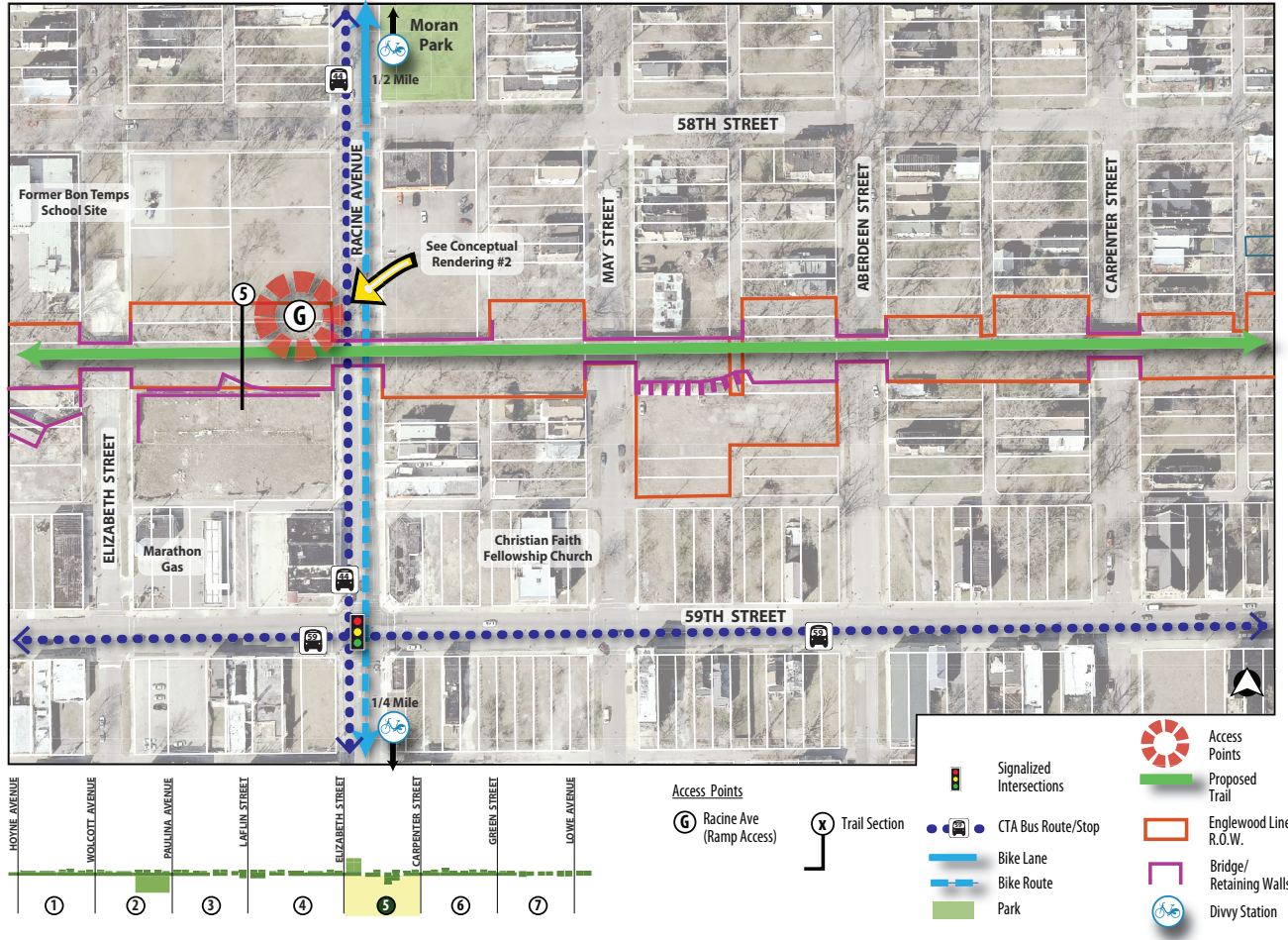


CONCEPT SECTION 4

4. Between Loomis Boulevard and Ada Street



5: ELIZABETH ST. TO CARPENTER ST.



CONCEPT SECTION 5

5. Between Elizabeth Street and Racine Avenue



CONCEPTUAL RENDERING # 2

Access point at Racine Avenue near former Bon Temps School



6: CARPENTER ST. TO GREEN ST.

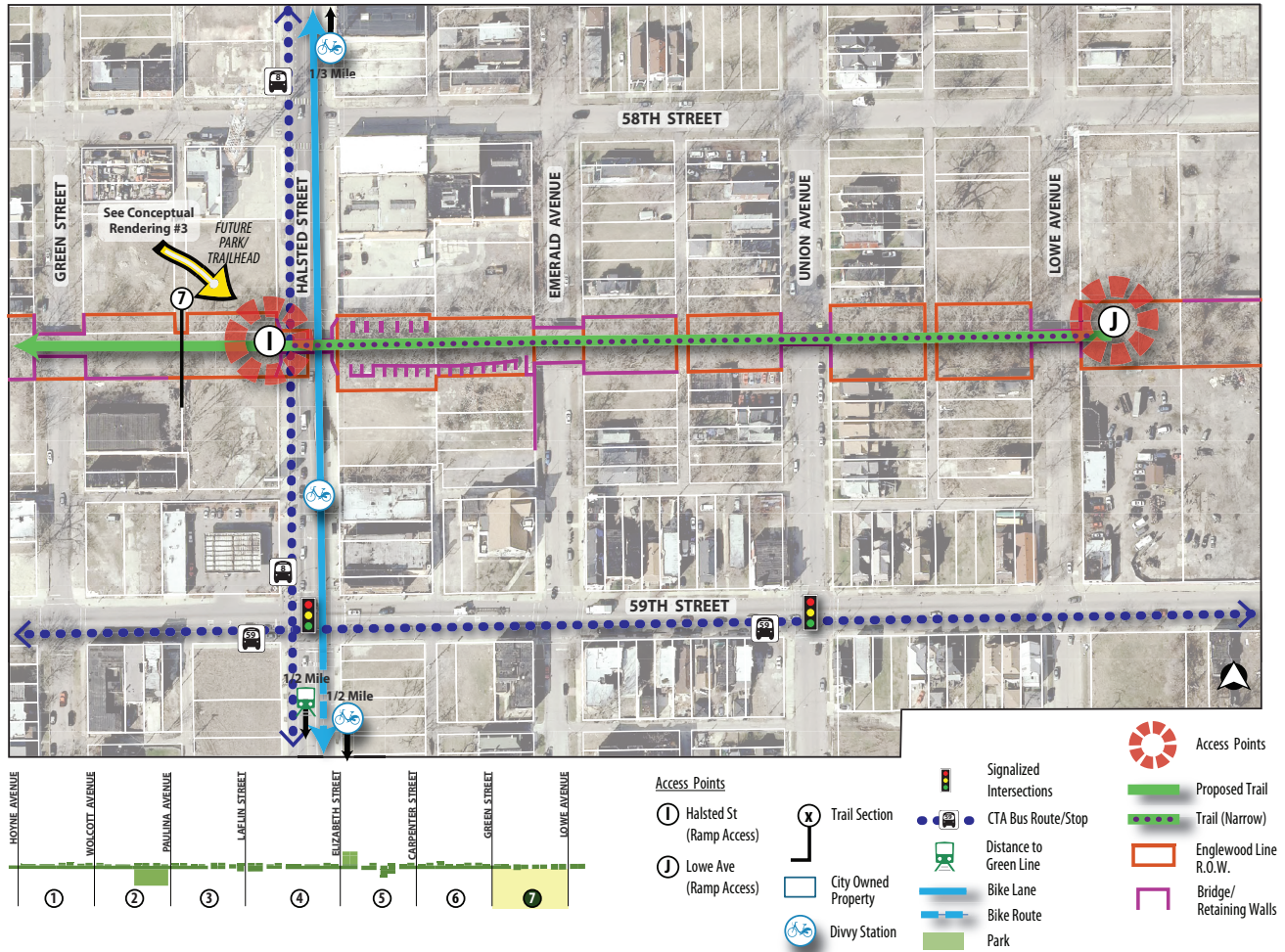


CONCEPT SECTION 6

6. Between Morgan Street and Sangamon Street



7: GREEN ST. TO LOWE ST.



CONCEPT SECTION 7

7. Between Green Street and Halsted Street



CONCEPTUAL RENDERING # 3

Access Point at Halsted Street with Proposed Market Space



APPENDIX D

Project Coordination

Prepared For:

Chicago Department of Transportation
Division of Engineering

**Chicago Department of Planning
and Development**

Prepared By:



CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No.
Contract (PO) No. 30531
Specification No. 116875
Initial Project Scoping Meeting
Wednesday, February 11, 2013, 10:00 am to 11:00 am
Attendees: see attached sign in sheet

Meeting Items:

1. General Overview

CDOT stated that the project will be overseen by both the Department of Transportation and Department of Planning and Development. Luis Benitez and Moira Coughlin will be the point of contact for the Department of Transportation. Nelson Chueng will be the point of contact for the Department of Planning and Development. This project is part of a pending transaction between the City of Chicago and Norfolk Southern Railroad (NS) for a transfer of lands between the two entities.

This Task Order will not be a full Phase I, but instead a feasibility study to determine the condition of the existing structures and any inherent liabilities the City may be burdened with by taking ownership of the structures. A separate environmental Task Order administered by 2FM is underway to access the land between the structures.

All work must be done in conformance with IDOT Phase I procedures in the event that the project is elevated to a traditional Phase I.

CDOT does not have nor have they requested from NS existing plans of the structures. The rail has been removed from the tracks and no trains have operated on the connector track in a long time, so it is not believed that NS has any current bridge inspection reports of the structures. The property was owned by CONRAIL prior to NS's acquisition of CONRAIL. The point of contact with NS is Kelly Harris. Patrick is to contact NS to request available existing plans and inspection reports for the structures.

POST MEETING: NS, Kelly Harris, responded to Patrick's information request for existing structure plans and/or bridge inspection reports in 2 separate emails on Thursday 02/12/2015. NS does not have bridge inspection reports OR existing plans for the 26 structures.

2. Structural Inspection Requirements

The City has an agreement in place with NS that authorizes the City to access the Englewood Connector Railroad Corridor. Patrick and their sub-consultant will be authorized under this agreement to perform the necessary field inspections. Patrick does not need to obtain a Right-of Entry (ROE) agreement with NS. However, Patrick and their sub-consultants will be responsible to ensure their insurance is in conformance with NS requirements. The City will provide Patrick a copy of the insurance requirements.

PI Surveying Group (PI) is currently performing a survey of the corridor for the City as part of a separate contract. The survey information will be provided to Patrick. Additional survey may be necessary. For the purpose of scoping and budgeting of this Task Order, an arbitrary place holder value will be used in the CECS for surveying. If after review of the PI survey data and the field inspections are complete, Patrick may request CDOT to authorize Dynasty, our sub-consultant, to perform pick-up survey utilizing the designated budget set aside for pick-up survey.

The structural inspection is to be visual inspection of the bridges and retaining walls adjacent to the abutments (parallel to the roadway). A hands-on inspection of the bridges is not required. Non-destructive testing will not be required. Sounding of the structures is to be performed on a limited basis. Equipment such as a man lift is not required for the inspection. If traffic control is required to perform the inspection, Patrick shall obtain all necessary permits from the City. Mike Simon is the point of contact for any lane closure permits. It is thought that performing the inspections at off peak times will be sufficient and that lane closure may not be required. If vehicular access to the top of the bridges is required, an existing access point is located near Halsted Street.

The structures should be inspected and the analysis should be performed with the understanding that the structures will be for pedestrian use only, with the infrequent use by emergency equipment to provided medical assistance if needed. The analysis should consider AASHTO loading requirements for pedestrian structures with a live loading of 100psf and an H-10 vehicle loading.

Concrete cores of the structures may be required depending on the conditions of the structures as determined during the field inspections. For the purposes of the scoping and budgeting of this Task Order, an arbitrary direct cost should be used to cover selective coring and testing of the samples. Patrick will verify with a vendor that a reasonable cost is included in the CECS in order for 10-20 cores to be taken and tested.

It is understood that the top of the bridge superstructure and any waterproofing membrane may not be visually inspected due to existing ballast. The ballast shall not be removed as part of the inspection. Therefore, inspection of the superstructure may only entail visually inspecting the underside of the deck.

No inspection below existing grade of the bridge substructures and retaining walls is required.

The inspection and inspection reports shall follow IDOT and NBIS inspection policies.

As part of the potential repurposing of the existing track to recreational trail, CDOT intends to minimize construction on the bridges. The existing bridges will be repair/rehabilitate, as necessary. CDOT does not intend to replace any of the bridges, as long as conditions allow.

The inspection should entail review of vertical clearance signing within the proximity of each bridge. If vertical clearance is not posted, Patrick is to notify the City. Also, Patrick is to notify Luis directly regarding any items observed during the inspection that require immediate attention.

3. Schedule

The project with CDOT's approval of the final deliverable is needed by the end of 2015; however, by midsummer CDOT wants to know the condition of the bridges and anticipate scope of work for the bridges based on condition.

4. Deliverables

The final submittal shall be in the form of an abbreviated BCR, per the IDOT guidelines and procedures. The submittal shall be to a level of effort that it could be sent on to IDOT for review if the project were to be advanced to a Phase I. The structures shall not be submitted piece meal, but shall be submitted as a single document to CDOT.

5. Project Administration

Patrick will attend bi-weekly meetings at CDOT during the project to discuss the project's status. Meetings are anticipated to be brief in nature to provide an update on current project status and discuss any issues that may have come up.

Patrick will prepare a draft scope and cost proposal using IDOT's BLR CECS form. The scope and price proposal will be submitted to CDOT by the end of the first week of March.

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 01
Thursday, July 16, 2015, 9:00 am to 9:30 am
Attendees: see attached sign in sheet

Meeting Items:

1. General Project Administration

This project will have regularly scheduled bi-weekly project coordination meetings. The meetings will be held at CDOT's office on the 4th floor. The next meeting will be Thursday, July 30th at 9:00 AM.

Patrick is to put together a project schedule showing key dates, task durations, and the anticipated inspection/design task sequence.

Patrick will verify with Norfolk Southern (NS) whether or not a right-of-entry (ROE) agreement is required to perform the inspection. Per discussions during the scoping of the Task Order, it is anticipated that the License Agreement between the NS and the City of Chicago will serve as the ROE for the inspection work.

Patrick will also verify if the NS will require any special safety training in order to work on NS property.

2. Structural Inspection

CDOT requested that the structural field inspection be completed as soon as possible. The office work associated with the field inspections (preparation of reports and inspection exhibits) should be minimized until after a plan-in hand field meeting is held and coordination with CDOT regarding the project deliverables has occurred.

Patrick anticipates starting the inspection after receiving the as-built drawings for the bridges from the NS. Patrick has already contacted G&A Consulting Engineers (G&A), one of the 3 firms contracted by the NS, who is authorized to go to NS's office in Atlanta, pull the aperture cards and reproduce the drawings. G&A will provide Patrick with pdf files (1 file per drawing) of the as-built drawings within 3 weeks from today.

The initial inspection is anticipated to last approximately two weeks. The inspections will be performed by two teams, one led by Patrick and one led by Infrastructure. The two teams will work simultaneously starting at opposite ends of the project.

CDOT directed Patrick to capture enough information during the initial inspection to complete the required bridge information sheet CDOT may submit in order to request a bridge number to be assigned to each structure. CDOT will provide a copy of the bridge information sheet to Patrick prior to the start of inspections.

The initial bridge inspections will be completed without traffic control.

The initial inspection of the retaining walls does not need to include any detailed documentation of concrete spalls or crack mapping. The focus of the initial inspection is to assess the walls' overall structural condition and the approximate level/cost of repairs anticipated.

Upon completion of the initial inspections, a plan-in-hand, field meeting will be held with Patrick and CDOT to review the findings from the initial inspections. Patrick will be responsible for identifying and showing CDOT structural deficiencies that may require more than "minor work".

After the plain-in-hand field meeting has been completed, CDOT will make the "Go or No Go" assessment for moving forward with the project. If in the opinion of CDOT the structures require significant repairs, the project may be stopped and not carried forward; however, that is not anticipated, and after the field review, the format for the Bridge Condition Reports (BCR) will be confirmed.

Additional inspection may be required after the "Go" decision has been confirmed by CDOT in order to complete the BCRs. Traffic control may be obtained to complete the additional inspection.

The BCRs are anticipated to follow the same format as the reports performed for the Bloomingdale Trail project. CDOT will provide Patrick with a sample report from the Bloomingdale Trail project to verify the potential format.

The structures that were used to support the connecting spur tracks need to be inspected; however, it is anticipated that these columns/foundations/piers will be removed. The inspection will be visual and document the structure for the purpose of being able to generate a cost estimate for its removal.

3. Environmental Inspection

The environmental inspection scoped for this task order should be performed as soon as possible. Patrick will arrange for the environmental testing to begin soon.

CDOT will provide the information or a means to obtain the previously performed environmental Phase I & II evaluation for the areas between the bridges. The previous evaluation identified one known area of environmental risk.

4. Aerial Mosaics

Work associated with the aerial mosaics will be delayed until after the plan-in-hand field review meeting.

5. **Action Items**

Ownership	Action Item	Status	Anticipated Completion Date
Patrick	Submit Detailed Project Schedule	In progress	07/23/2015
Patrick	Confirm with NS whether or not a ROE is required or if the existing License Agreement is sufficient	Email sent to Kelly Harris (NS) on 07/16/15	07/23/2015
Patrick	Confirm with NS whether or not safety training certification is needed prior to entering NS property	Completed. N. Chueng confirmed no e-railsafe training is needed since it is not an active line	07/16/2015
CDOT	Provide bridge information check sheet	In progress	07/30/2015
CDOT	Provide example BCR from the Bloomingdale Trail Project	In progress	07/30/2015
CDOT	Provide Phase I & II Environmental Findings for areas between the bridges	In progress	07/30/2015

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 02
Thursday, July 30, 2015, 9:00 am to 9:30 am
Attendees: see attached sign in sheet

Meeting Items:

1. Project Schedule

Patrick provided an overview of the project schedule that had been submitted to CDOT prior to the start of the meeting. The project is anticipated to be completed with the final Abbreviated BCR's (ABCRs) submittal in December of 2015 before the Christmas holiday.

2. Environmental Inspection

The environmental inspection scoped for this task order is currently being performed in the proximity of the bridges. The lead based paint testing is anticipated to be completed within two (2) days and a brief report summarizing the findings will be submitted to CDOT.

CDOT provided to Patrick the environmental Phase I & II reports for the areas between the bridges. Patrick will review the environmental data to obtain any information that may pertain to the structures.

3. Project Data

Prior to the meeting CDOT provided an excel file containing the bridge data required to obtain a Structure Number for each bridge. CDOT also provided a sample ABCR from the Bloomingdale Trail to utilize as a template for this project.

Patrick is working to obtain the existing structure as-built drawings. Patrick was informed by the local consultant that the cards for 197 of the 208 requested drawings were located, have been pulled and are in the process of being copied. Patrick is to receive the as-built plans in the near future, sort the data, and draft existing plan and elevation view exhibits to utilize during the field inspection and incorporate into the ABCR.

4. Action Items

Ownership	Action Item	Status	Anticipated Completion Date
Patrick	Perform environmental inspection in the proximity of the bridges	In progress	08/13/2015
Patrick	Obtain as-built plans, sort data by bridge, and draft existing plan and elevation view exhibits	In progress	08/17/2015

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 03
Thursday, August 13, 2015, 9:00 am to 9:40 am
Attendees: see attached sign in sheet

Meeting Items:

1. Bridge Inspection

Patrick received the available as-built drawing information from Norfolk Southern (NS) on Wednesday 08/12/2015. A CD containing the files as they were received by Patrick was given to CDOT for their records.

Patrick anticipates starting the field inspections on Monday 08/17/2015.

Patrick presented the IDOT Bridge inspection forms that are intended to be completed in the field as part of the inspections. It was noted that IDOT has a newer version of the inspection report that is formatted differently and requires less detailed information than the older form that was used and included in the Bloomingdale sample report provided by CDOT. Patrick would prefer to use the older version of the inspection report. CDOT concurred that the older form with additional information is better and complete during the inspection; however, CDOT will confirm if the current version must be the version submitted to IDOT.

Patrick intends to standardize the photos included in the BCR and provided the intended photo lists to be used at each inspection. There will not be as many pictures included in the BCR as the example BCR from the Bloomingdale Trail project CDOT provided.

There will be two (2) bridge inspections teams working simultaneously. CDOT requested that Patrick provide the contact information for each team (name of team lead and cell phone number to be used to get in touch with the team while they are in the field), starting location and general plan of inspection. The inspection teams if questioned by police or authorities are to provide Luis Benitez names and number as client contact.

The "sketch-in hand" field meeting will be at 9AM on Thursday 09/10/2015. The starting point for the meeting will be determined after the initial inspections have been completed. It is assumed that the field meeting will require approximately four (4) hours.

CDOT clarified that a single abbreviated BCR (ABCR) is to be completed and submitted in advance of the other ABCRs. The initial ABCR will be coordinated with IDOT to ensure

the content and format is acceptable to CDOT and IDOT. Upon concurrence from CDOT and IDOT, the remaining ABCRs will be completed.

2. Environmental Inspection

The field work associated with lead-based paint (LBP) inspection of the bridge structures that are part of this task order has been completed. Patrick is waiting to receive the results from the analysis. The analysis is anticipated to be completed in about a week. Upon Patrick’s receipt of the analysis, Patrick will compile a brief report summarizing the findings to be submitted to CDOT.

CDOT asked that Patrick provide the name of the vendor performing the LBP testing, the price per structure, and a summary of the testing procedure being completed. The information is needed for another project.

3. Survey

The City indicated that the survey to be provided is still approximately four (4) weeks from being available.

4. Action Items

Ownership	Action Item	Status	Anticipated Completion Date
Patrick	Perform environmental inspection in the proximity of the bridges	Field work completed. Analysis is in progress. Summary report to be compiled once the analysis has been received.	08/21/2015
Patrick	Obtain as-built plans, sort data by bridge, and draft existing plan and elevation view exhibits	COMPLETED. Information presented at the meeting	08/13/2015
Patrick	Provide LBP Testing information to CDOT	COMPLETED. Emailed over to CDOT on 08/13/2015	08/13/2015
Patrick	Provide inspection team contact information to CDOT	In progress.	08/14/2015
CDOT	Confirm which version of the IDOT inspection form is to be used.	In Progress	08/20/2015
CDOT	Provide corridor survey	In Progress	09/10/2015

SIGN-IN SHEET

Project Name: Englewood Connector Railroad Bridges
E-5-482

Meeting Date: 8/13/15

Meeting Name: Bi-weekly No 3

Meeting Time: 9:00AM

Meeting

Location: CDOT 4A

Name:	Company Name	Email Address
-------	--------------	---------------

Ken Smorynski I E I ksmorynski@infrastructure-eng.com

Sarah Szaplicki Patrick szaplicki@patrickco.com

Luis D. Benitez CDOT luis.Benitez@cityofchicago.org

Moira Coughlin CDOT moira.coughlin@cityofchicago.org

Nelson Chueng PPD nchueng@cityofchicago.org

Steve Lynch Patrick slynch@patrickco.com

PANICAJ KUMAR I E I pkumar@infrastructure-eng.com

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 04
Thursday, September 10, 2015, 9:00 am to 11:50 am
Attendees: see attached sign in sheet

Field Inspection Meeting Items:

1. The purpose of the meeting was for City representatives to see first-hand in the field the initial finding of the completed bridge inspections. City representatives were able to make an assessment of whether or not any significant structural deficiencies exist that would result in the project not being carried forward.
2. A safety briefing was given by Patrick prior to the start of the field inspection.
3. The first structure reviewed was Racine Avenue railroad structure over the roadway.
4. The City mentioned that the project if carried forward may take advantage of "Green Core", a City sponsored organization that could potentially remove trees, as necessary, from the project site.

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 05
Thursday, September 17, 2015, 9:00 am to 9:50 am
Attendees: see attached sign in sheet

Meeting Items:

1. Abbreviated Bridge Condition Report

Patrick provided hard copies of a draft sample Abbreviated BCR (ABCR). The report is based upon the example from the Bloomingdale Trail project previously provided by CDOT and has been slightly modified based on IDOT's bridge condition report templates.

CDOT has requested approximately a day to review and provide any general content related comments. Patrick will address any comments and provide an electronic version of the report. The report will be forwarded onto the IDOT Bridge Office. A conference call will then be arranged with the Bridge Office to confirm IDOT's requirements for this particular project.

It is acceptable to submit the BCR with the structure number noted as to be determined (TBD), unless the structure has previously been assigned a structure number. Patrick is to verify if any of the bridges already have been assigned a structure number.

A brief narrative is to be added explaining Norfolk Southern's current ownership of the structures and CDOT's pending acquisition of the structures.

The project/route name used in the ABCRs will be "Englewood Line".

The ABCRs will be submitted to IDOT without the inclusion of the cost estimate. The project will submit twenty-six (26) individual ABCRs to IDOT for their review.

2. Project Deliverable

As part of the overall project deliverable, Patrick will determine the jurisdiction ownership of the roadways under the Englewood Connector bridges.

The format of the final project deliverable will be discussed at a future meeting.

3. Mosaics

CDOT will provide additional guidance on the information that they expect to see on the project mosaics. The mosaics will incorporate files related to the Englewood Connector project that were prepared for the City by another consultant. Work on the Mosaics will wait until additional guidance from CDOT is provided.

4. Survey

The City indicated that the survey to be provided is not completed.

5. Action Items

Ownership	Action Item	Status	Anticipated Completion Date
Patrick	Perform environmental inspection in the proximity of the bridges	Field work completed. Analysis is in progress. Summary report to be compiled once the analysis has been received.	08/21/2015
CDOT	Provide corridor survey	In Progress	09/10/2015
CDOT	Provide comments on draft ABCR	In Progress	09/18/2015
Patrick	Provide revised draft ABCR for CDOT to forward to IDOT	In Progress	09/22/2015
Patrick	Patrick to verify if any of the structures have been assigned a structure number.	In Progress	10/01/2015
Patrick	Provide two (2) discs with the NS existing bridge plans to CDOT	In progress	10/01/2015
CDOT	Provide guidance on what information CDOT wants to see displayed on the mosaics	In Progress	10/01/2015

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 06
Thursday, October 15, 2015, 4:00 pm to 4:30 pm
Attendees: Moira Kent, Sarah Czaplicki, and Steve Lynch

Meeting Items:

1. Structure Vertical Clearance

Patrick provided a hard copy (electronic pdf provided post-meeting) of the summary table depicting all of the structures vertical clearances. On the table, the type of existing signage present at each structure for the vertical clearance is indicated.

2. Bridge Structure Number Request

Patrick provided in a table format a hard copy (electronic pdf provided post-meeting) of the information necessary to request a structure number to be assigned to each of the bridges. Patrick verified that none of the bridge structures have been assigned a structure number previously.

3. Abbreviated Bridge Condition Report

Patrick provided hard copies of a draft Abbreviated BCR (ABCR) for Racine Avenue. (Electronic pdf provided post-meeting).

CDOT to verify the content of the cost estimate information to be included in the report. Discussion at the meeting was for the cost estimate to be limited to only the bridge structure items and to contain language stating that only bridge structure items were included. Patrick to prepare of list of the potential pay items to be included in the cost estimate.

4. Lead Based Paint Testing

Patrick has received the 26 individual lead based paint testing reports. CDOT requested the information be submitted for review. The reports indicate that all 26 locations have some amount of lead-based paint..

5. Miscellaneous

CDOT directed Patrick to verify the jurisdiction of all routes the bridge structure cross over. Particularly Halstead and Ashland as those are thought to be IDOT in some parts of the City.

6. Action Items

Ownership	Action Item	Status	Anticipated Completion Date
Patrick	Perform environmental inspection in the proximity of the bridges	Completed. Information submitted to CDOT post meeting via email.	10/15/2015
CDOT	Provide corridor survey	In Progress	09/10/2015
CDOT	Provide comments on draft ABCR	In Progress	09/18/2015
Patrick	Provide revised draft ABCR for CDOT to forward to IDOT	In Progress. Revised ABCR for Racine Avenue provided to CDOT for review.	09/22/2015
Patrick	Patrick to verify if any of the structures have been assigned a structure number.	Completed. No structure Nos. have been assigned to any of the bridges.	10/01/2015
Patrick	Provide two (2) discs with the NS existing bridge plans to CDOT	Completed.	10/01/2015
CDOT	Provide guidance on what information CDOT wants to see displayed on the mosaics	In Progress	10/01/2015

**CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 06
Thursday, October 22, 2015, 9:00 am to 10:00 am
Attendees: See Attached Sign-In Sheet**

Meeting Items:

1. Abbreviated Bridge Condition Report

Discussion of the previously submitted Racine Avenue ABCR. CDOT comments:

All reference to the substructure shall remain general no specifics. The City has not yet determined a funding source for the project. Without any immediate funding requests, CDOT is unable to definitely provide direction on the inclusion of any non-structural cost items, such as lighting, drainage, roadway improvements or street level sidewalks.

The ABCR should contain a cost estimate. The cost estimate must clearly indicate on it that cost are “only structural improvements”.

Based on CDOT’s previous project experience the quantity for spalled concrete should be increased by 30%.

2. IDOT Kick-off meeting

In order for the Englewood Line project to eligible for federal funding for the Phase II, III, and Construction, the project must be coordinated with IDOT. CDOT will arrange for the initial kick-off meeting with IDOT. CDOT will provide a power point presentation template to Patrick. Patrick to populate the template in preparation for the meeting.

Items still to be determined are the project termini and overall general project scope.

3. Action Items

Ownership	Action Item	Status	Anticipated Completion Date
CDOT	Provide corridor survey	In Progress	09/10/2015
CDOT	Provide comments on draft ABCR	Completed.	10/22/2015
Patrick	Provide revised draft ABCR for CDOT to forward to IDOT	In Progress. Revised ABCR for Racine Avenue provided to CDOT for review.	09/22/2015
CDOT	Provide guidance on what information CDOT wants to see displayed on the mosaics	In Progress	10/01/2015

CDOT	Provide bid tabs and plan set from Bloomingdale Trail project for use in the cost estimate.	In Progress.	10/30/2015
Patrick	Prepare Power Point presentation for IDOT Kick-off meeting	In progress	11/06/2015

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 08
Wednesday, January 20, 2016, 1:00 pm to 2:30 pm
Attendees: see attached sign in sheet

Meeting Items:

1. Abbreviated Bridge Condition Report (ABCR)

CDOT provided comments regarding the 2 ABCRs previously provided (Elizabeth Street and Lowe Avenue) prior to start of conference call with IDOT.

CDOT's Comments:

Regarding Lowe Avenue:

- CDOT takes no exception to the level of detail in the ABCR.
- Confirm with IDOT on what size location map is required (During call with IDOT, confirmed that no additional location map is needed.
- The expected service life of the structure is to be adjusted to 50 years since the structure will not be exposed to salt, only experience pedestrian loading, pedestrian loading will not cause fatigue.
- The cost should be revised for the replacement to more accurately reflect the true replacement cost. Replacement cost included only shows cost for a new superstructure. However if the structure needs to be raised 4-6 feet to meet IDOT pedestrian bridge clearance requirements 17'-3", new abutments would be required and adjustment of the trail profile would be necessary. The revised cost will change the recommended structure to rehabilitation instead of reconstruct.
- Need to add a railing if the rehabilitation recommendation is carried forward. Currently not included.

IDOT's Comments

- All ABCR(s) require a structure number to be included in order to be reviewed.
- All new/reconstructed pedestrian bridges must have a vertical clearance of 17'-3".
- Rehabilitation of the existing structure and keeping the existing vertical clearance at 11'-2" will require a waiver from local roads.

2. General Project Information

Project will utilize local funding for the Phase I.

Project Termini have not yet been confirmed. CDOT is working to establish the termini.

ABCR needed for all walls greater than 10' from top of wall to bottom of footing (Local Roads criteria) or with an exposed wall height greater than 7' (Bridge manual).

3. Project Deliverable

The final deliverable for the Task Order will be a complete Project Development Report (PDR). This will complete the federal requirements for the Phase I and allow the project to be eligible for federal funding for the Phase II, Phase III and construction.

No aerial mosaics will be required. Muller & Muller (WBE) to be reassigned to help with the PDR in some capacity.

4. Survey

The City indicated that the survey to be provided is not completed.

5. Action Items

Ownership	Action Item	Status	Anticipated Completion Date
CDOT	Provide corridor survey	In Progress	09/10/2015
Patrick	Provide revised draft ABCR for CDOT to forward to IDOT	Completed.	12/18/2015
CDOT	Provide guidance on what information CDOT wants to see displayed on the mosaics	In Progress	10/01/2015
CDOT	Provide bid tabs and plan set from Bloomingdale Trail project for use in the cost estimate.	Completed	10/22/2015
Patrick	Prepare Power Point presentation for IDOT Kick-off meeting	Completed	11/24/2015
Patrick	Prepare level of effort summary documenting from where things stand currently, what it would take to complete the Phase I PDR for the full 1.7 mile stretch.	In progress	02/04/2016



MEETING ATTENDANCE RECORD

Project Name: Conditions Assessment of the Englewood Connector Railroad Bridges **Date:** January 20, 2016

Patrick Project Number / Task: 21577.401 **Meeting Location:** CDOT; Conference Room 4A

<u>NAME</u>	<u>COMPANY</u>	<u>PHONE / EXT.</u>	<u>E-MAIL ADDRESS</u>
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CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 09
Tuesday, April 05, 2016, 10:00 am to 10:40 am
Attendees: see attached sign in sheet

Meeting Items:

1. Abbreviated Bridge Condition Reports (ABCR) for Bridge Structures

CDOT will review Section IV's recommendation of the rehabilitation alternative versus the replacing the through-plate girder superstructure alternative for the 4 steel bridge structures. CDOT will provide comments to Patrick by end of day tomorrow (Wednesday).

CDOT will confirm the number of copies of the ABCRs that IDOT will require for the ABCR review submittal.

CDOT will provide to Patrick a copy of the draft Transmittal Letter on CDOT stationary for the ABCR submittal to IDOT for Patrick's records.

Patrick will deliver the ABCRs to CDOT by Monday (04/11/2016) at 8:30 AM. CDOT will deliver the ABCRs to IDOT.

The bridge structures that currently have a substandard vertical clearance height and are being rehabilitated with the same vertical clearance height will need a waiver from the IDOT Bureau of Local Roads & Streets. The waiver will be obtained as part of the Phase I Project Development Report (PDR) and not the ABCR process.

2. ABCRs for Retaining Wall Structures

Per earlier project coordination with IDOT, it was confirmed that ABCRs are only needed for retaining walls with an exposed height greater than 7ft. There are total of 7 retaining walls with an exposed height greater than 7ft within the project corridor.

A template to be used for the retaining wall ABCRs was provided for review. The ABCRs for the 7 retaining walls are in progress. The retaining wall ABCRs will be submitted to IDOT separately from the bridge structure ABCR submittal.

Auxiliary structures, remnants of private service tracks, are stand-alone structures independent of the trail retaining walls or bridges. The auxiliary structures will not be reused in a structural capacity. Therefore, ABCRs will not be prepared for the auxiliary structures. There are approximately 6 of these structures within the project limits. The structures do not present an obvious safety hazard. The future incorporation of the

structures into the Englewood Line aesthetics will be determined based on input from the stakeholder groups involved.

3. Phase I – Project Development Report (PDR)

A working draft of the Supplemental scope for the completion of the Phase I PDR is being reviewed by the City. Comments regarding the level of effort included in the scope will be provided to Patrick.

Patrick is to provide to the City an order of magnitude cost for the Supplement. The Supplement would be a reallocation of funds already committed to the Task Order in addition to any additional funds required to complete the Supplement. Cost will be approximate until the scope can be agreed to by the City.

Patrick may add an additional M/WBE subconsultant to the Task Order Supplement if the subconsultant is already on Patrick's Master DUR contract. The added subconsultant would not be authorized to begin work until the Supplement has been authorized.

4. Other Items of Work

Patrick has reached out to Dynasty to obtain scope and pricing information regarding the completion of a full topographic survey for the project corridor. The pricing assumes that Dynasty will perform the survey controls.

The City indicated that the survey work being performed by PI Surveying is not complete. PI Surveying's scope does not include topographic survey and is related to establishing the project right-of-way.

Public involvement will be part of the Patrick PDR. The public involvement required for the PDR will be in addition to any of the on-going public involvement being performed by the City and their other consultant.

The City is currently working with Teska Associates, Inc. under a separate contract to study some of the potential trail access points and to assist with the on-going public involvement for the project. The City stated it was acceptable for Patrick to reach out to Teska to get an understanding of what work they have performed to ensure there is not a duplication of work being completed. The City indicated that a potential trail access point that is being considered is near Elizabeth Street. Project Termini have not been finalized, but Halsted Street is being evaluated for the eastern terminus.

The work Teska is performing is being funded through CMAQ and the funding goes through 2017.

5. **Action Items**

Ownership	Action Item	Status	Anticipated Completion Date
CDOT	Provide corridor survey	In Progress	TBD
Patrick	Submit Bridge ABCR to CDOT to forward to IDOT	In Progress	04/11/2016
Patrick	Prepare order of magnitude cost to complete the Phase I PDR for the full 1.7 mile stretch.	In progress	04/19/2016
CDOT	Provide a copy of the Bloomingdale Trail Phase I – PDR for reference	In progress	04/19/2016

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 10
Tuesday, October 25, 2016, 1:00 pm to 2:00 pm
Attendees: see attached sign in sheet

Meeting Items:

1. Abbreviated Bridge Condition Reports (ABCR) for Bridge Structures

The comments from IDOT Central Bureau of Bridges and Structures – Local Bridge Unit (IDOT) date June 28, 2016 and transmitted to Patrick on August 31, 2016 were discussed.

Patrick to provide a draft disposition of the comments prior to the next project status review meeting to CDOT for their review. Upon receiving CDOT's concurrence of the disposition, the disposition will be sent to IDOT along with any ABCRs that were revised to address the comments. Only ABCRs that have been revised will be resubmitted.

Comments regarding evaluating the cost of bridge replacement verses bridge rehabilitation will be addressed with an explanation. Based on previous coordination with IDOT, any bridge replacement would require the new structure to have a 17'-3" vertical clearance in accordance with IDOT Bureau of Local Roads for pedestrian bridges over highways. Therefore, any bridge replacement costs would need to include new abutments, retaining walls and embankment to increase the trail profile approximately 6' to ensure the replacement structure satisfies this vertical clearance requirement. The existing structures do not meet current vertical clearance requirements; however, rehabilitation of the structures would allow the structure to maintain their present vertical clearances. The magnitude of the additional work associated with the bridge replacement is significantly greater than bridge rehabilitation scope of work. Per previous direction from CDOT, replacement costs were not be included in the ABCRs since this alternative is cost prohibitive.

The BLR 10220 Asbestos Determination Certification for Local Highway Bridges will be submitted after the test result from the bridges' waterproofing membranes have been received. Test results are anticipated in the first half of December.

Previous discussion at the April 5, 2016 meeting was that the vertical clearance waivers for the existing bridges will be obtained as part of the Phase I Project Development Report and not the ABCR process. This remains current.

IDOT's comment regarding tree removal will be interpreted that tree removal scope or cost shall be removed from the ABCRs cost estimates since that activity is a general project requirement and not directly related to the structural work.

The modification of the vertical stiffeners detail from the 606 Trail project will be used to address IDOT's comments regarding the stiffeners as potential snag points for the future path.

ABCRs for Retaining Wall Structures

ABCRs for all existing walls greater than 7 ft. and functioning in a structural capacity will be submitted.

A few wall ABCRs will be submitted to CDOT for their review prior to preparing the submittal to IDOT. Patrick to provide sample wall ABCRs by 11/04/16.

ABCRs will be not prepared for auxiliary structures, walls or structures currently not providing structural support to the trail or its embankment. These structure locations will be defined in a simple exhibit. The future incorporation of these structure into the Englewood Line project will be determined during the PDR.

2. Phase I – Project Development Report (PDR)

The City asked Patrick to revise the previously submitted PDR scope proposal to address the work recently authorized. The east-west project limits will remain the same as previously scoped; however, the north-south limits will be expanded. The City will provide to Patrick an approximate area, including trail access points and associated parks, to be covered by the Phase I Study.

Patrick is to provide to the City with the revised scope for review prior to preparing a fee estimate. The scope of work as applicable should incorporate the work DPD's sub-consultant is currently doing for the Englewood Line.

The City is considering submitting an application to receive CMAQ funds for the Englewood Line. CMAQ applications are due in late summer 2017.

3. Other Items of Work

The survey schedule was distributed. Survey crews are in the field completing the topographic survey. CDOT/CDPD to confirm with NS if NS will require track centerlines, RR monuments or other items incorporated into the plats and legals in order to facilitate NS's review and acceptance of the plats and legals.

The bridge deck core field work started on Monday this week; however, completion of two cores and change in equipment is proposed to complete the remainder of the cores. The work will resume late next week or early the following next week using a small drill rig. The field work will take approximately a week. Lab results of the testing on the waterproofing membrane takes approximately 2 weeks and will begin after all bridge deck cores have been taken. Lab results are anticipated to be received in the first half of December.

CDOT/CDPD stated they had received a feasibility study for the "New Century Trail" from the Alderman. The study area includes the proposed Englewood Line and was put together independently of any or the projects currently being done by CDOT or DPD.

4. Action Items

Ownership	Action Item	Status	Anticipated Completion Date
Patrick	Submit Disposition to IDOT Comments on the Bridge ABCRs	In Progress	11/04/2016
Patrick	Submit sample wall ABCR for CDOT to review	In progress	11/04/2016
Patrick	Submit updated draft Phase I PDR scope document to CDOT	In Progress	11/04/2016
CDOT/CDPD	Confirm with NS if additional information needed on survey plats and legals	In progress	11/04/2016
CDOT/CDPD	Provide approximate project boundary limits	In progress	11/09/2016
CDOT/CDPD	Provide electronic copy of the New Century Trail study	Completed	10/25/2016

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 11
Wednesday, November 09, 2016, 1:00 pm to 2:00 pm
Attendees: see attached sign in sheet

Meeting Items:

1. Disposition to IDOT Comments on the Bridge ABCRs

Patrick's draft responses to the IDOT BCR comments dated June 28, 2016 were discussed. No exception was taken to the comment responses.

The south Emerald Avenue BCR has the highest rehabilitation cost of all the structures. IDOT recommend a cost comparison to evaluate the superstructure replacement versus rehabilitation. The existing structure has 3 bays crossing Emerald Avenue and Patrick recommended removal of 2 of the bays to be consistent with the other bridge widths and to reduce the overall structure, minimizing future maintenance costs for the City. At this stage of the project it was agreed that the BCR will be for the rehabilitation of all 3 bays of the existing bridge and that there will be no removal. During the Phase I Study, if the proposed structural scope at Emerald Avenue changes, the BCR will be revised as part of the PDR and a separate memo will be sent to IDOT to document the change. The Emerald Avenue BCR will be revised per the discussion above and resubmitted to IDOT with the disposition of comments.

The detail from the Bloomingdale Trail/606 project to cut the existing steel thru-girder vertical stiffeners will be included with the disposition. The detail will not be added to the BCRs.

No other BCRs need to be revised.

2. Retaining Wall Condition Reports

CDOT's comments regarding the sample retaining wall condition reports submitted were discussed. The table of contents will not be modified and the content that is not included will be denoted as not-applicable to acknowledge that it was intentionally omitted and not left out by error.

Four copies of all the RWCRs are to be turned in to CDOT by the end of the week. CDOT will submit the reports to IDOT District 1 on Monday as part of a previously planned meeting.

3. Phase I Project Development Report (PDR) Scope

The auxiliary structures that have not been covered by the bridge condition reports or retaining wall condition reports were discussed. It was agreed that the stand alone

auxiliary structures will not be included in the PDR. These structures are anticipated to be part of a separate project or incorporated into the project during the phase II. No scope of work will be included for the stand alone auxiliary structures.

Future trail access locations are anticipated to need a retaining wall / structure to retain embankment and provide ADA access from street level to the trail. The scope will include an agreed to number of hours to develop TS&L plans for the proposed structures at the access points. The scope will assume 6 access locations.

Based on the Emerald Avenue BCR discussion (see item 1), additional hours will be included to prepare TS&L drawings for Emerald Avenue.

TS&L plans for the bridge repair locations are not required.

The scope will include the necessary public involvement to obtain design approval of the PDR. The public involvement anticipates 3 separate public meetings (one meeting in each of the three aldermanic wards that the project is within). There will not be a public hearing. Patrick will include Teska on the Phase I project team to assist with Public Involvement. The organization Grow Greater Englewood has been assisting on CDPD's Englewood Project. Patrick will work with Teska to try to utilize them as part of the Phase I public involvement.

4. Project Schedule

CDPD anticipates submitting the Englewood Project for federal Transportation Alternatives Program (TAP) funding. The TAP funding application is due in late June. Prior to submitting the application, the PDR including a cost estimate, needs to have been submitted to or coordinated with IDOT, so that design approval is received prior to TAP funding being awarded.

5. Action Items

Ownership	Action Item	Status	Anticipated Completion Date
Patrick	Organize meeting with Teska, CDPD, CDOT and Patrick to coordinate project scope	In Progress	11/16/2016
Patrick	Submit updated draft Phase I PDR scope and CECS documents to CDOT	In progress	11/18/2016
CDOT/CDPD	Confirm with NS if additional information is needed on survey plats and legals	In progress	11/18/2016
CDOT/CDPD	Provide approximate project boundary limits	In progress	11/16/2016



MEETING ATTENDANCE RECORD

Project Name: Conditions Assessment of the Englewood Connector Railroad Bridges

Date: November 17, 2016

Patrick Project Number / Task: 21577.401

Meeting Location: CDOT; Conference Room 4A

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**CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531**

Transportation Alternatives Program Funding Application - Discussion

Friday, January 06, 2017, 3:00 pm to 4:00 pm

Attendees: Nelson Chueng (DPD), Jeff Sriver (CDOT-Planning), Keith Privett (CDOT-Planning),
Moira Kent (CDOT-Engineering), Jarrod Cebulski (Patrick), & Steve Lynch (Patrick)

The purpose of the meeting was to discuss preparing and submitting the Englewood Line's (59th Street railroad trail conversion) request for funding application to the Chicago Metropolitan Agency for Planning (CMAP) for consideration through the Congestion Mitigation and Air Quality Improvement (CMAQ) program and the locally programmed Transportation Alternatives Program (TAP-L).

The funding application schedule is as follows:

1. Application booklet and related materials made available - January 6, 2017
2. Complete DRAFT application submitted internally to Keith Privett - February 15, 2017
 - a. DRAFT application is to have been reviewed and revised accordingly by the project team prior to being sent on to Keith
3. Complete Application submitted to CMAP March 3, 2017

The application is to focus on having a clear defined project scope of work. The scope narrative can briefly discuss the project's potential social, economic and political approach in the first paragraph. The second and third paragraphs of the application narrative are to be the detailed scope of work specifics (i.e., length and width of trail, number of bridges involved, etc.). The final paragraph should tie the project back to the funding policy. Teska should be solicited for input in writing the first and last paragraphs of the application contextual narrative. Exhibits, such as a project location map and other bicycle plans, as well as site photos, should be included with the overall application.

The scope narrative is to be for the full limits of the project, as there is no perceived benefit of shortening or extending the project limits to different termini. The funding will only the physical trail aspects of the project and items such as aesthetics and landscaping will not be eligible.

The project budget is to be as specific as possible in order to demonstrate the completeness of the Phase I study. Avoid the use of lump sum items and instead use each items when the level of design is not advanced far enough. If necessary, cost contingencies can be included in the budget submitted as part of the application.

The planned project schedule should assume the TAP-L funding would be available in March 2018. Project milestone dates should reflect a timeline consistent with the IDOT approval process for the Project Development Report (PDR). The public meeting does not need to occur before the June 1, 2017 deadline.

Key points to be considered:

1. The frequency of access points should be in line with what was constructed for the Bloomingdale Trail and not the frequency shown on the Bloomingdale Trail's CMAQ application.
2. The project may primarily function as a recreational trail, but the application should discuss the transportation aspects of the trail, as well.

Action Items

Ownership	Action Item	Status	Anticipated Completion Date
CDOT-Planning	Provide Bloomingdale Trail CMAQ application as an example	In Progress	1/17/2017
Patrick	Review CDOT's "Streets for Cycling" plan to ensure the project incorporates existing or planned bike routes for inclusion in the application.	In Progress	1/20/2017
Patrick	Submit 1 st DRAFT narrative and budget	In Progress	02/01/2017

CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 14
Wednesday, March 15, 2017, 2:30 pm to 3:45 pm
Attendees: See attached sign-in sheet.

The purpose of the meeting was to present the status of the design progress and to establish the accepted design criteria to be used for the project.

Meeting Items:

1. Design Criteria

- a. Multi-Use Path (Trail): The design is to be per the AASTHO Guide for the Development of Bicycle Facilities (4th Edition, 2012). All structural elements associated with the trail design shall be based upon the AASTHO Standard Specification for highway Bridges (17th edition) or LRFD.

The proposed width of the path was discussed. The design is proposing a 12-ft wide hard pavement path with 2-ft of soft pavement (aggregate) along each side of the trail. The trail must maintain a minimum of 5-ft from edge of hard pavement on either side of the trail with a grade flatter than 6:1 or a railing must be provided. The trail's hard pavement will be bituminous.

CDOT/DPD discussed potentially replacing the 2-ft soft pavement with an extended hard pavement to mitigate future maintenance. The width of the proposed hard pavement is to be confirmed. A potential trail section width of 10-ft or 12-ft continuous asphalt surfacing was discussed.. The benefits of a reduced trail section would be tree and landscape preservation.

Patrick to prepare a design criteria summary specifying the trail width. The summary will include a brief project description, design criteria and a few pictures of the existing trail to depict the existing trail conditions. CDOT/DPD will circulate the design criteria document to confirm trail width. The use of soft pavement edges along the trail is to be verified.

- b. ADA Standards: The City of Chicago's ADA guidelines will apply to the project. Access ramps will be designed with a maximum grade of 5% to eliminate flat landings on the access ramps.
- c. Drainage Requirements: The approving agency for the Location Drainage study will be the Chicago Department of Water Management (CDWM). Infrastructure

will coordinate directly with CDWM, keeping CDOT and Patrick copied on correspondence with CDWM.

The Location Drainage Study will be prepared using the IDOT format. IDOT is not anticipated to provide a technical review of the LDS.

- d. Trail Lighting: The lighting along the trail will follow Division of Electrical Operations (DEO) standards. The lighting standards will be included on the summary design criteria being prepared by Patrick.

2. Trail Concept Plan

Teska provided a draft of the overall trail concept including the access points. The anticipated ROW to be acquired by the City was shown along with significant adjacent features Divvy stations, CTA, business and parks.

3. Supplemental Topographic survey

Patrick stated that the supplemental topographic survey for the access points has been identified and provided to the surveyor. The surveyor is in the process of scheduling the field work to be performed.

4. Plats of survey

Patrick stated that the Plats of Survey exhibit requested by Greater Illinois Title to resolve the discrepancies between title descriptions has been provided to Greater Illinois Title. Greater Illinois Title is working to review the exhibit and has not requested a meeting or any additional information at this time. The ROW from Damen to Hoyne had not previously been shown since it was outside of the original project limits. Patrick is working with Dynasty to reallocate project funds to complete the ROW investigation for this area.

5. Other Agency Coordination

- a. Chicago Park District: The City will arrange for a meeting with the Chicago Park District to introduce the project and to verify the design criteria.
 - b. Chicago Office of Emergency Management and Communications (OEMC): Patrick will prepare a draft letter to introduce the project and solicit input from OEMC on the project development.
 - c. CTA, Police, Fire: A draft letter introducing the project will be sent to the agency, but no further coordination is expected with the agencies is expected.
6. The next project status meeting will be in 2 weeks, March 29, 2017 in the afternoon.

Action Items

Ownership	Action Item	Status	Anticipated Completion Date
Patrick	Prepare design Criteria document	In Progress	3/17/2017
Teska	Provide electronic copy of the meeting exhibits	Completed	
Patrick	Prepare draft letter to OEMC introducing the project	In Progress	03/29/2017
DPD	Arrange a meeting with the Chicago park District	In Progress	3/29/2017
IEI	Arrange a meeting with the CDWM	In progress	3/29/2017



MEETING ATTENDANCE RECORD

Project Name: Englewood Line

Date: March 15, 2017

Patrick Project Number / Task: 21577.401

Meeting Location: CDOT Conference Room 4C

<u>NAME</u>	<u>COMPANY</u>	<u>PHONE / EXT.</u>	<u>E-MAIL ADDRESS</u>
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CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 15
Wednesday, April 05, 2017, 3:00 pm to 4:00 pm
Attendees: See attached sign-in sheet.

The purpose of the meeting was to update the City regarding the status of the design and to solicit initial comments/input regarding the preliminary trail geometry.

Meeting Items:

1. Design Criteria

- a. General: Patrick submitted to CDOT and DPD the anticipated Multi-Use Path (Trail) design criteria document. The document submitted has been revised to include the trail lighting design criteria. Patrick is waiting for Infrastructure (IEI) to provide the drainage design criteria prior to resubmitting the final design criteria document. CDOT and DPD will circulate the document once the revised version is submitted.
- b. Trail Width: The design criteria recommends a trail width based on the anticipated volume of users of the trail during the peak hour. If there are more than 300 anticipated users during a peak hour, the trail is recommended to be 12 FT wide. Less than 300 users during a peak hour and the trail width is recommended to be 10 FT wide. DPD requested Patrick to provide the number of trees impacted by a 12 FT wide trail as compared to a 10 FT wide trail. Patrick will consider all trees within the required recoverable slope adjacent to the pavement as impacted trees.
- c. Drainage Design: A meeting was held with the Building Department private development sewers review team on 03/28/2017. Infrastructure presented the drainage design approach and the Building Department provide feedback and some general guidance on what they will need to approve. An additional meeting with Department of Water Management public sewers group will also be necessary. Building Department requires \$3,000.00 fee to review the project documents. Coordination with the Building Department may be moved to the Phase II design in order to coordinate the Building Department's review with the project's permit application.

2. Miscellaneous – Field Report

- a. DPD informed the design team that the western termini of the trail near Hoyne Avenue had recently been cleared of all trees. The work was not performed by the City or NS. It is not know who cleared the trees, but it appeared that the site is being prepared for additional work to be performed on it.

3. Trail Access Ramp Preliminary geometry

Exhibits showing the preliminary geometry for all trail access points were provided and discussed. Unless noted otherwise, all trail access points are anticipated to be 10 FT wide.

- a. Hoyne Avenue – No comments. The existing area is open and at-grade so the design is very flexible. A trail head with limited parking is anticipated to the north of the trail along Hoyne. This access will be 12 FT wide to accommodate emergency vehicle access to the trail.
- b. Damen Avenue – No comments. The preliminary design will require a retaining wall to minimize impacts to private property. The acquisition of 2 additional lots was discussed to eliminate the need for the wall and to create greater space for the trail access. It was decided not to pursue additional ROW acquisition at this site.
- c. Hermitage Park (South) – No comments. Area is open and the trail geometry could be refined during Phase II if necessary without changing the anticipated impacts. A temporary access is anticipated to be built at this location by Green Core in summer 2017. The temporary access will not be restricted by a gate.
- d. Hermitage Avenue (North) – Patrick and Teska to coordinate their current design concepts. Access trail will originated from Hermitage Avenue and connect to the Englewood Line near Wood Street. A proposed retaining wall is anticipated. A temporary access is anticipated to be built at this location by Green Core in summer 2017. The temporary access will not be restricted by a gate.
- e. Ashland Avenue – No comments. It was discussed that the trail access orgin being closer to 58th Street was acceptable as it allows pedestrians to utilize the cross walks at the intersection of Ashland Avenue with 58th Street to safely cross from the west side of Ashland to the east side where the trail access is located.
- f. Loomis Boulevard – No comments. The trail will impacts an existing retaining wall under 7 FT.
- g. Former Bon Temps School – The preliminary design will be revised. Patrick and Teska to coordinate design efforts at this location. It was agreed that the primary ADA trail access will come from Racine Avenue to ensure the project is able to accommodate the Knight Plan presented by Alderman Lopez. A sidewalk will connect from Elizabeth Street to Racine to allow ADA access from the future redevelopment of the school. A stair access will also be provided from Elizabeth Street straight to the trail. Previous temporary access was built at this location by Green Core in summer of 2016. Temporary access is restricted by a gate that was existing prior to the temporary access being built.

- h. Morgan Street – No Comment. The preliminary design will require a retaining wall to minimize impacts to private property.
 - i. Halsted Street (North) – The preliminary design will be revised. The access trail at-grade termini will be moved south closer to the Englewood Line to allow for additional development on the City owned property.
 - j. Halsted Street (South) – An additional switchback access ramp is to be added to the SW quadrant at Halsted Street.
 - k. Lowe Street - The preliminary design will be revised. The access ramp is to either use a switchback alignment or stairs to minimize the ramp’s footprint.
4. **Supplemental Topographic survey**
- Patrick stated that the supplemental topographic survey for the access points has been completed and is being processed by the surveyor. Patrick anticipates receiving the information within the next day or so.
5. **Plats of survey**
- Dynasty Group and Greater Illinois Title (GIT) are scheduling a meeting for the end of this week/early next week (04/10) to resolve technical discrepancies and resolve comments regarding the Plat. A second meeting involving the City legal and NS legal will be schedule after Dynasty and GIT have resolved the technical discrepancies. The second meeting will be to discuss the format for preparing the written legal description.
6. **Other Agency Coordination**
- a. Chicago Park District: The City will arrange for a meeting with the Chicago Park District to introduce the project and to verify the design criteria.
 - b. Chicago Office of Emergency Management and Communications (OEMC): Patrick will prepare a draft letter to introduce the project and solicit input from OEMC on the project development.
7. The next project status meeting will be in 2 weeks, April 19, 2017 in the afternoon.

Action Items

Ownership	Action Item	Status	Anticipated Completion Date
Infrastructure	Prepare drainage design Criteria document	In Progress	04/15/2017
Patrick	Prepare draft letter to OEMC introducing the project	In Progress	03/29/2017
DPD	Arrange a meeting with the Chicago park District	In Progress	3/29/2017
Patrick	Provide estimated number of impacted trees for a 10 FT vs. 12 FT wide trail	In progress	4/15/2017



MEETING ATTENDANCE RECORD

Project Name: Englewood Line

Date: April 05, 2017

Patrick Project Number / Task: 21577.401

Meeting Location: CDOT Conference Room 4A

<u>INITIALS</u>	<u>NAME</u>	<u>COMPANY</u>	<u>PHONE / EXT.</u>	<u>E-MAIL ADDRESS</u>
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	Sarah Czaplicki	Patrick Engineering	(630) 795-7318	sczaplicki@patrickco.com
	Jarrold Cebulski	Patrick Engineering	(630) 795-7468	jcebulski@patrickco.com
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	Harish Goyal	Infrastructure Engineering	(312) 425-9560 x1249	hgoyal@infrastructure-eng.com
SG	Scott Goldstein	Teska Associates	(847) 563-9724	SGoldstein@TeskaAssociates.Com
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**CITY OF CHICAGO
DEPARTMENT OF TRANSPORTATION
DIVISION OF ENGINEERING
Conditions Assessment of the Englewood Connector Railroad Bridges
CDOT No. E-5-482
Contract (PO) No. 30531
Project Coordination Meeting No. 16
Wednesday, April 19, 2017, 2:30 pm to 3:30 pm
Attendees: See attached sign-in sheet.**

The purpose of the meeting was to update the City regarding the status of the design and to solicit initial comments/input regarding the preliminary trail geometry.

Meeting Items:

1. Design Criteria

Patrick is waiting to submit to CDOT and DPD the revised Multi-Use Path (Trail) design criteria document until the drainage design criteria has been included. Infrastructure (IEI) to provide the drainage design criteria ASAP. CDOT and DPD will circulate the document once the revised version is submitted.

2. Trail Access Ramp Preliminary geometry

Exhibits showing the preliminary geometry for all trail access points and additional rendering of Hermitage and Bon Temps accesses were provided and discussed.

- a. Hoyne Avenue – No comments on the horizontal geometry. No revisions will be made to the horizontal geometry at this time.
- b. Damen Avenue –The access ramp geometry is to be revised to keep the access ramp oriented more parallel to the main trail and eliminate tangents perpendicular to the main trail. The access is to be moved closer to the trail embankment to reduce the access’ footprint and reduce the land required to construct the access. An additional retaining wall is anticipated to accommodate the access’ new layout. The only ROW to be acquired are the 4 lots SE of the trail along Damen. The access trail is to be re-laid out as to limit its encroachment on to the required additional ROW to allow for some of the additional ROW to be used as park space.
- c. Hermitage Park (South) – No comments on the horizontal geometry. No revisions will be made to the horizontal geometry at this time.
- d. Hermitage Avenue (North) – A stair access is to be added to the ramp to provide a more direct path up the embankment to connect to the trail. The ADA complaint access ramp will remain as shown with the addition of stairs being placed in line with the access ramps’ street side starting point.

- e. Ashland Avenue – No comments on the horizontal geometry. No revisions will be made to the horizontal geometry at this time. An additional non-ADA (Stairway) access is to be added to the SW corner of Ashland and the trail. The new stairway will require modification to the existing bridge abutment wall.
- f. Loomis Boulevard – No comments on the horizontal geometry. No revisions will be made to the horizontal geometry at this time.
- g. Former Bon Temps School – Teska presented a concept for the access. There were no comments on the horizontal geometry presented as mirrors the concept developed by Teska with the ADA access being from Racine, which is the preferred approach.
- h. Morgan Street – No comments on the horizontal geometry. No revisions will be made to the horizontal geometry at this time.
- i. Halsted Street (North) – No comments on the horizontal geometry. No revisions will be made to the horizontal geometry at this time.
- j. Halsted Street (South) – An additional stair access will be added to allow for a more direct access to the trail from the sidewalk south of the trail on the east side of Halsted. The access ramps from the north and south side are to be modified to intersect the main trail at or close to the same station. The south side access ramp's point of intersection with the main trail will be modified to align with the north side access ramp's layout.
- k. Lowe Street - No comments on the horizontal geometry. No revisions will be made to the horizontal geometry at this time.

3. Plats of survey

Dynasty Group and Greater Illinois Title (GIT) met and have resolved a majority of the discrepancies between the title commitment and existing legal description's. A second meeting involving the City legal and NS legal is to be schedule to discuss the format for preparing the written legal description.

The project plats will not involve any land west of Hoyne Avenue.

4. Structural Design

- a. CDOT to follow-up with IDOT to see the status of the review of the previously submitted BCRs for the retaining wall and bridge structures.
- b. The scope of the project is to provide TS&L drawings for the proposed retaining wall. The boring logs from the environmental site assessment do not show any of the soil bearing capacity information needed for the TS&L drawings. The structural retaining wall drawings will be modified to show all of the same information as the TS&L, except for the geotechnical information.

5. Other Agency Coordination

- a. Chicago Park District: The City will arrange for a meeting with the Chicago Park District to introduce the project and to verify the design criteria.
 - b. The next FHWA coordination meeting is May 8, 2017. If the project were to be presented all meeting materials would need to be submitted by May 4, 2017. Patrick to see if preparation of the meeting materials is feasible so that the initial coordination meeting with FHWA can take place prior to June 1.
6. The next project status meeting will be in 2 weeks, May 03, 2017 in the afternoon.

Action Items

Ownership	Action Item	Status	Anticipated Completion Date
Infrastructure	Prepare drainage design Criteria document	In Progress	04/15/2017
Patrick	Prepare draft letter to OEMC introducing the project	In Progress	03/29/2017
DPD	Arrange a meeting with the Chicago park District	In Progress	3/29/2017
Patrick	Provide estimated number of impacted trees for a 10 FT vs. 12 FT wide trail	Completed. Email out on 04/20/2017	



MEETING ATTENDANCE RECORD

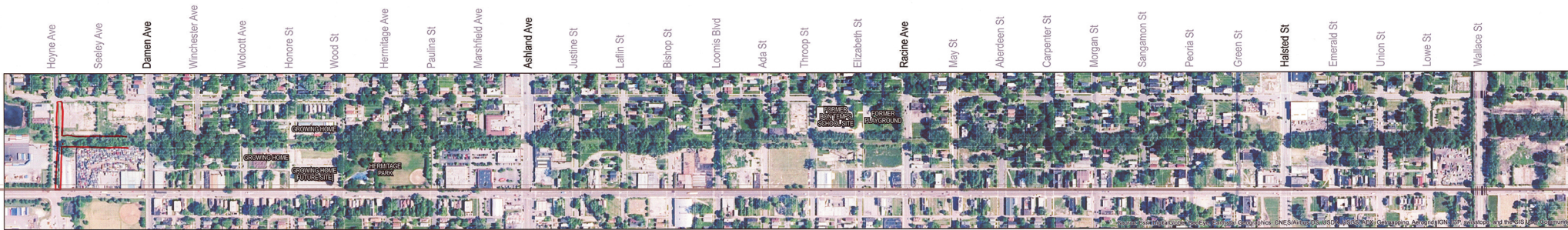
Project Name: Englewood Line

Date: April 19, 2017

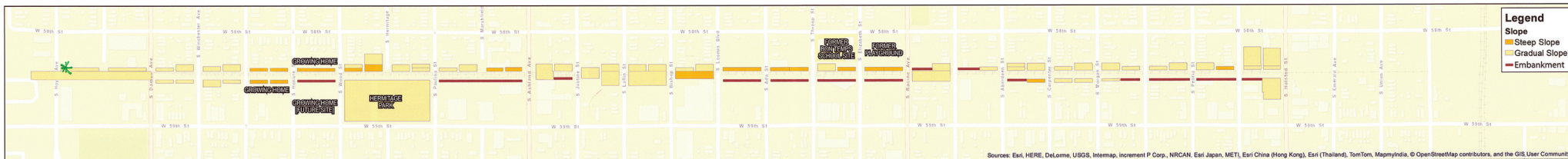
Patrick Project Number / Task: 21577.401

Meeting Location: CDOT Conference Room 4A

<u>INITIALS</u>	<u>NAME</u>	<u>COMPANY</u>	<u>PHONE / EXT.</u>	<u>E-MAIL ADDRESS</u>
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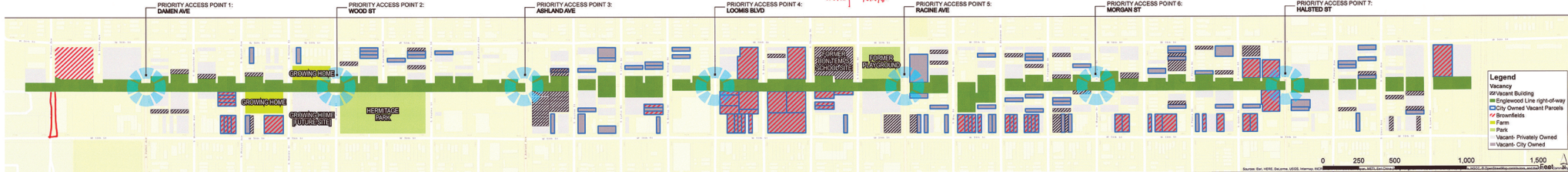
Aerial map



Map showing type of slopes along the trail



Existing land use map



Vacancy, brownfield, and City-owned parcels map





MEETING ATTENDANCE RECORD

Project Name: Englewood Line

Date: May 25, 2017

Patrick Project Number / Task: 21577.401

Meeting Location: CDOT Conference Room 4A

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